

Meeting called to order by the Mayor
Invocation and Pledge of Allegiance

1. Consent Agenda:

- a. Approve Minutes – August 6, 2019 Regular Session
- b. Approve Tax Releases
- c. Approve Request from Graham First United Methodist Church to close East Market Street from 3:00 p.m. – 8:00 p.m. on Sunday, October 27, 2019 for a Community Event, provided the following conditions are met prior to the event:
 - i. Provide a Certificate of Liability insurance meeting all City requirements (received)
 - ii. Schedule public safety personnel following the Extra Duty Solutions process

2. Old Business:

- a. Public Hearing: Petition for Voluntary Contiguous Annexation for a 111.44 acre area off Rogers Road (GPIN 8872986536 & 8873900485) (AN1901):
 - i. Approve Annexation Ordinance
- b. Approve Mutual Aid Memorandum of Understanding with Alamance County for Alamance County Detention Center Evacuation
- c. Boards & Commissions Task Force Update

3. Requests and Petitions from Citizens:

- a. Encroachment License Agreement Request by Paul Crotts to extend the front façade at 135 South Main Street

4. Recommendations from Planning Board:

- a. Public Hearing: RZ1902 Roll Ward Street. Application by Jensen Roll to rezone approximately 0.25 acres from R-12 to R-7 (GPIN 8884130286)
- b. Public Hearing: RZ1903 Albany Street Duplex. Application by Cheaper Homes LLC to rezone approximately 0.25 acres from B-2 to R-7 (GPIN 8874850468)
- c. Public Hearing: AM1903 Banquet Hall. Application by Aneesa Glines for a text amendment to permit banquet halls with the business and industrial districts
- d. Public Hearing: SUP1902 Moore St Townhomes. Application by Brad Deaton for a Special Use Permit for 61 townhomes (GPIN 8883460674, 8883364585, 8883367692) **Item tabled by Planning Board 8/20/19**

5. Downtown Small Area Plan:

- a. Adopt Downtown Master Plan

6. Code of Ordinances:

- a. First Reading: Approve Ordinance amendment to CHAPTER 18- STEETS, SIDEWALKS AND OTHER PUBLIC PLACES, ARTICLE IV- EXCAVATION PERMIT of the Code of Ordinances to amend the requirements and permitting process for excavation in the City Right-of-Way
- b. First Reading: Approve Ordinance amendment to CHAPTER 12- OFFENSES AND MISCELLANEOUS PROVISIONS, ARTICLE III- NUISANCES of the Code of Ordinances to delete requirement of the City to store removed junk debris for seven days

7. Closed Session Pursuant to the Terms of N.C.G.S. §. 143-318-11 (a) (5): to establish, or to instruct the public body's staff or negotiating agents concerning the position to be taken by or on behalf of the City Council in negotiating (i) the price and other material terms of a contract or proposed contract for the acquisition of real property by purchase, option, exchange, or lease.

8. Issues Not on Tonight's Agenda

**CITY OF GRAHAM
REGULAR SESSION
TUESDAY, AUGUST 6, 2019
7:00 P.M.**

The City Council of the City of Graham met in regular session at 7:00 p.m. on Tuesday, August 6, 2019, in the Council Chambers of the Municipal Building located at 201 South Main Street.

Council Members Present:

Mayor Jerry Peterman
Mayor Pro Tem Lee Kimrey
Council Member Chip Turner
Council Member Griffin McClure
Council Member Melody Wiggins

Also Present:

Frankie Maness, City Manager
Aaron Holland, Assistant City Manager
Darcy Sperry, City Clerk
Bryan Coleman, City Attorney
Mary Faucette, Downtown Development Coordinator
Tommy Cole, Fire Chief

Mayor Jerry Peterman called the meeting to order and presided at 7:00 p.m. Mayor Peterman gave the invocation and everyone stood to recite the Pledge of Allegiance.

Consent Agenda:

- a. Approve Minutes – July 2, 2019 Regular Session*
- b. Approve Minutes – July 9, 2019 Special Session*
- c. Approve Tax Releases*

| CITY OF GRAHAM RELEASE ACCOUNTS | | | | |
|--|-------------|---------------------------|---|----------------------------|
| AUGUST | | | | |
| <u>ACCT #</u> | <u>YEAR</u> | <u>NAME</u> | <u>REASON FOR RELEASE</u> | <u>AMOUNT RELEASED</u> |
| 521775 | 2019 | BV 170 LLC | RE IN WRONG NAME - DISCOVERED IN CORRECT NAME | 2,880.59 |
| 25144 | 2019 | ANDRESS, CHARLES LOGAN | QUALIFIED FOR HOMESTEAD EXEMPTION | 179.43 |
| 443659 | 2019 | SLOMINSKI, FREDERICK L | QUALIFIED FOR DISABLED VETERAN EXCLUSION | \$204.75 |
| 665048 | 2019 | SPRINKLE, ROBERT HARRY | PROPERTY SOLD BEFORE 7/1, REMOVE EXEMPTION AND BILL FULL TAX BILL TO NEW OWNER | \$498.17 |
| 678089 | 2019 | CASHION, JOHN EDWARD III | DOES NOT LIVE IN CITY OF GRAHAM-BOAT | \$68.76 |
| 678174 | 2019 | REICHENBACH, KENNETH WADE | BOAT HAD WRONG VALUE | \$163.57 |

- d. Approve Tax Collector's Year End Report*
- e. Approve 2018 Outstanding Tax Listing*
- f. Approve Resolution Authorizing Conveyance of a Chandler and Price Flatbed Printing Press to Elon University Pursuant to G.S. 160A-280*

**RESOLUTION AUTHORIZING CONVEYANCE OF A CHANDLER AND PRICE
FLATBED PRINTING PRESS TO ELON UNIVERSITY PURSUANT TO G.S. 160A-280**

WHEREAS, the City of Graham was donated a Chandler and Price 10" x 15" flatbed printing press through the operation of the Graham Historic Museum; and

WHEREAS, Elon College originally purchased the Chandler and Price 10" x 15" flatbed printing press in 1938 from a Concord North Carolina newspaper. Because of their quick set-up and low production costs, small flatbed or "jobbing" presses, like this widely used Chandler and Price, were a cost effective way to print limited numbers of good quality smaller pieces. It was the second press purchased by the college and was used to print many items that had been printed off campus such as letterhead stationery, event programs, college forms, and tickets.; and

WHEREAS, Louis Jones and Jerry Peterman worked to preserve and restore the printing press; and

WHEREAS, North Carolina General Statute § 160A-280 authorizes the donation of any surplus personal property, including supplies, materials, and equipment, that the governing board deems to be surplus, obsolete, or unused; and

WHEREAS, the Graham Historical Museum Advisory Board recommends that the Chandler and Price 10" x 15" flatbed printing press be deemed surplus and that it be donated to Elon University.

THEREFORE, THE GRAHAM CITY COUNCIL RESOLVES THAT:

1. The Chandler and Price 10" x 15" flatbed printing press is hereby declared surplus; and
2. The City of Graham hereby conveys to Elon University the Chandler and Price 10" x 15" flatbed printing press subject to agreement that the preservation and restoration efforts of the Graham Historical Museum, Louis Jones and Jerry Peterman be acknowledged.
3. The property herein described shall be conveyed for no consideration.
4. The City Manager, Finance Officer and City Clerk are authorized to execute all documents necessary to convey the property in the manner authorized by this Resolution.

Adopted this 6th day of August, 2019.

- g. Approve Mutual Aid Memorandum of Understanding with Alamance County for Alamance County Detention Center Evacuation*
- h. Approve Request from ALCOVETS to close the 100 block of East Elm Street from 5:00 p.m. on Friday, September 13, 2019 through 5:00 p.m. on Sunday, September 15, 2019 for the 2019 DockDogs event, provided the following conditions are met prior to the event::*
 - i. Provide a Certificate of Liability insurance meeting all City requirements*
 - ii. Submit a Temporary Outdoor Sales Permit Application*
 - iii. Schedule public safety personnel following the Extra Duty Solutions process*

Mayor Peterman asked Council Members if they would like to pull any of the items from the Consent Agenda. Council Member Melody Wiggins asked to pull item "g", while Mayor Pro Tem Lee Kimrey asked to pull items "d" and "e".

Council Member Chip Turner made a motion to approve items "a", "b", "c", "f" and "h" on the Consent Agenda, seconded by Council Member Wiggins. All voted in favor of the motion.

With respect to item “d”, Mayor Pro Tem Kimrey asked City Manager Frankie Maness what the delinquent tax amount equates to with regard to our tax rate. Mr. Maness stated it would be approximately $\frac{3}{4}$ of a cent on our tax rate. With no further questions forthcoming, Mayor Pro Tem Kimrey made a motion to approve item “d” on the Consent Agenda, seconded by Council Member Griffin McClure. All voted in favor of the motion.

With respect to item “e”, Mayor Pro Tem Kimrey asked Mr. Maness how the federal government could owe the City of Graham tax money on real estate. Mr. Maness guessed there might have been an acquisition, potentially a foreclosure, for a property that remained taxable at the time they gained ownership. With no further questions forthcoming, Mayor Pro Tem Kimrey made a motion to approve item “e” on the Consent Agenda, seconded by Council Member Griffin McClure. All voted in favor of the motion.

With respect to item “g”, Council Member Wiggins stated that she believes there may be another memorandum of understating already in place for this facility – possibly with the City of Burlington. She asked that staff look into that. She also expressed concern with moving inmates from a secured facility to an unsecured facility. Mayor Pro Tem Kimrey stated that he would like to see an exit clause included in the agreement. With no further comments forthcoming, Mayor Pro Tem Kimrey made a motion to postpone this agenda item until next month’s meeting, seconded by Council Member Wiggins. All voted in favor of the motion.

Mr. Tom Boney of the Alamance News stepped forward and asked about item “f” on the Consent Agenda. He inquired as to its origin, how long the City had it and who took action to remove it. Mayor Peterman advised that Elon College was the original owner. The City received the printing press and placed it in the museum. Council Member Wiggins confirmed that by an official vote, the current Historical Museum Advisory Board made the suggestion to the City Council to give it back to Elon College.

Requests and Petitions from Citizens:

a. Encroachment License Agreement Request by John Zheng for a proposed grease trap at 139 West Elm Street

Assistant City Manager Aaron Holland explained that John Zheng has submitted a written request and supporting photos for a proposed grease trap to be installed within the sidewalk at 139 W. Elm Street. He added that the intent of the grease trap is to serve a future restaurant at the location. Additionally, an encroachment license agreement approved by City Council is required pursuant to Sec. 18-2(c) of the City’s Code of Ordinances. Mr. Holland stated that City staff has met and concurs with the proposed location of the grease trap and Historic Resources Commission approval is not required for this installation. He further added that there is a nominal expense to the City for costs associated with processing and monitoring the license and advised that staff recommends approval.

Following a brief discussion between Council Members and staff, Council Member Wiggins made a motion to authorize the City Manager and City Attorney to enter into an encroachment license agreement with John Zheng for a proposed grease trap at 139 W. Elm Street as identified by the submitted photos with the following condition(s):

- Provide proof and maintenance of \$1,000,000 in liability insurance with the City as additional insured.

- Provide a final survey to confirm approved use of public space.
- Improvements under the Encroachment License must be made prior to the expiration of the building permit.

Council Member McClure seconded the motion and all voted in favor of the motion.

b. Petition for Voluntary Contiguous Annexation for a 111.44 acre area off Rogers Road (GPIN 8872986536 & 8873900485) (AN1901):

- i. Approve Resolution Requesting City Clerk to Investigate Sufficiency*
- ii. Approve Resolution Fixing Date of Public Hearing on Question of Annexation*

Mr. Holland advised that this is a corrective measure to the approved annexation request submitted by Shugart Homes at last month's meeting. Approval was subsequently rescinded due to the sale of the property by Shugart to Clayton Properties Group. The City has received a request to annex in 111.44 acres on Rogers Road. Tonight's requested action is the first step in a multiple step annexation process.

Council Member Turner asked if there had been any changes to the request, other than the petitioner. Mr. Holland stated there had not. With no further comments forthcoming, Council Member McClure made a motion to approve the Resolution Directing the Clerk to Investigate a Petition Received Under G.S. 160A-31 for a 111.44 acre area on Rogers Road, seconded by Council Member Wiggins.

Council Member McClure made a motion to approve the Resolution Fixing Date of Public Hearing on Question of Annexation Pursuant to G.S. 160A-31 for a 111.44 acre area on Rogers Road, the date being September 3, 2019. Council Member Turner seconded the motion and all voted in favor of the motion.

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| <p><u>RESOLUTION DIRECTING THE CLERK TO INVESTIGATE</u> <u>A PETITION RECEIVED UNDER G.S. 160A-31</u> <u>FOR A 111.44 ACRE AREA OFF ROGERS ROAD (AN1901)</u></p> |
| <p>WHEREAS, a petition requesting annexation of an area described in said petition was received on July 31, 2019, by the Graham City Council; and</p> |
| <p>WHEREAS, G.S. 160A-31 provides that the sufficiency of the petition shall be investigated by the City Clerk before further annexation proceedings may take place; and</p> |
| <p>WHEREAS, the City Council of the City of Graham deems it advisable to proceed in response to this request for annexation.</p> |
| <p>NOW, THEREFORE BE IT RESOLVED, by the City Council of the City of Graham:</p> |
| <p>That the City Clerk is hereby directed to investigate the sufficiency of the above described petition and to certify as soon as possible to the City Council the result of her investigation.</p> |

RESOLUTION FIXING DATE OF PUBLIC HEARING
ON QUESTION OF ANNEXATION PURSUANT TO G.S. 160A-31
FOR A 111.44 ACRE AREA OFF ROGERS ROAD (AN1901)

WHEREAS, a petition requesting annexation of the contiguous area described herein has been received; and

WHEREAS, the City Council has by resolution directed the City Clerk to investigate the sufficiency of the petition; and

WHEREAS, certification by the City Clerk as to the sufficiency of the petition has been made;

NOW, THEREFORE, BE IT RESOLVED, by the City Council of the City of Graham, North Carolina that:

Section 1. A public hearing on the question of annexation of the area described herein will be held at the City Hall, 201 S. Main Street, Graham, NC at 7:00 pm on September 3, 2019.

Section 2. The area proposed for annexation is described as follows:

ALL of that certain piece, parcel or tract of land lying and being in the City of Graham, Graham Township, Alamance County, North Carolina, and being more particularly described as follows:

DESCRIPTION IS OF THE DIXIE W. ROGERS ETAL PROPERTY AS RECORDED IN DEED BOOK 550, PAGE 473 OF THE ALAMANCE COUNTY REGISTER OF DEEDS AND BEING PARCEL ID # 130476 TOGETHER WITH A PORTION OF THE SHOFFNER LAND AND TIMBER, LLC PROPERTY AS RECORDED IN DEED BOOK 2512, PAGE 695 OF THE ALAMANCE COUNTY REGISTER OF DEEDS AND BEING PARCEL ID # 131641 AND IS AS FOLLOWS:

BEGINNING AT AN EXISTING IRON PIPE, SAID PIPE HAVING A NC NAD 83 (NSRS2011) COORDINATE OF N:830,321.28, E:1,878,218.84, SAID PIPE ALSO BEING A COMMON CORNER WITH THE JEAN S BONEY REVOCABLE TRUST PROPERTY AS DESCRIBED IN DEED BOOK 2519, PAGE 613; THENCE WITH THE EASTERN LINE OF THE JEAN S BONEY REVOCABLE TRUST PROPERTY

S 64°05'28" E FOR A DISTANCE OF 835.66 FEET TO AN EXISTING 1" PINCH TOP; THENCE CONTINUING WITH THE EASTERN LINE OF THE JEAN S BONEY REVOCABLE TRUST PROPERTY S 64°16'27" E FOR A DISTANCE OF 160.00 FEET TO A CALCULATED POINT IN THE CENTERLINE OF LITTLE ALAMANCE CREEK, SAID POINT BEING A COMMON CORNER WITH THE SHOFFNER LAND AND TIMBER, LLC PROPERTY AS DESCRIBED IN DEED BOOK 2512, PAGE 695; THENCE WITH THE SHOFFNER LAND AND TIMBER, LLC. PROPERTY S 64°16'27" E FOR A DISTANCE OF 335.12 FEET TO A POINT IN THE CENTERLINE OF LITTLE ALAMANCE CREEK; THENCE THE FOLLOWING TWENTYTWO (22) CALLS TO CALCULATED POINTS IN THE CENTERLINE OF LITTLE ALAMANCE CREEK,

- (1) N 55°15'21" E FOR A DISTANCE OF 54.94 FEET; THENCE
- (2) N 85°15'23" E FOR A DISTANCE OF 148.60 FEET; THENCE
- (3) N 40°16'02" E FOR A DISTANCE OF 115.52 FEET; THENCE

- (4) N 18°10'36" W FOR A DISTANCE OF 154.72 FEET; THENCE
 (5) N 12°52'15" W FOR A DISTANCE OF 256.88 FEET; THENCE
 (6) N 34°43'03" E FOR A DISTANCE OF 75.43 FEET; THENCE WITH THE MARTIN L. SHOFFNER PROPERTY AS RECORDED IN DEED BOOK 3440, PAGE 381,
 (7) S 81°45'47" E FOR A DISTANCE OF 169.58 FEET; THENCE
 (8) S 84°04'09" E FOR A DISTANCE OF 263.67 FEET; THENCE
 (9) S 80°26'01" E FOR A DISTANCE OF 114.19 FEET; THENCE
 (10) S 66°47'27" E FOR A DISTANCE OF 190.33 FEET; THENCE
 (11) S 37°41'56" E FOR A DISTANCE OF 209.62 FEET; THENCE
 (12) S 08°21'10" E FOR A DISTANCE OF 86.52 FEET; THENCE
 (13) S 04°35'47" W FOR A DISTANCE OF 50.86 FEET; THENCE
 (14) S 43°49'02" W FOR A DISTANCE OF 38.22 FEET; THENCE
 (15) S 82°19'35" W FOR A DISTANCE OF 146.52 FEET; THENCE
 (16) S 60°50'31" W FOR A DISTANCE OF 268.25 FEET; THENCE
 (17) S 05°47'09" W FOR A DISTANCE OF 8.69 FEET; THENCE
 (18) S 05°47'09" W FOR A DISTANCE OF 38.96 FEET; THENCE
 (19) S 33°53'21" E FOR A DISTANCE OF 143.51 FEET; THENCE
 (20) S 88°11'32" E FOR A DISTANCE OF 134.20 FEET; THENCE
 (21) S 38°14'39" E FOR A DISTANCE OF 142.86 FEET; THENCE
 (22) S 49°11'57" E FOR A DISTANCE OF 191.62 FEET;

THENCE LEAVING THE CENTERLINE OF LITTLE ALAMANCE CREEK ALONG THE SOUTHERN LINE OF THE FOUST FAMILY PROPERTIES, LLC PROPERTY AS DESCRIBED IN DEED BOOK 3520, PAGE 846 S 01°38'44" W FOR A DISTANCE OF 629.73 FEET TO AN EXISTING 1 ½" IRON PIPE, SAID PIPE BEING A COMMON CORNER WITH THE ETHEL C. ROGERS HEIRS PROPERTY AS DESCRIBED IN DEED BOOK 205, PAGE 536; THENCE WITH THE ETHEL C. ROGERS HEIRS PROPERTY THE FOLLOWING FOUR (4) CALLS,

- (1) N 88°54'32" W FOR A DISTANCE OF 131.76 FEET TO AN EXISTING 1 ½" IRON PIPE; THENCE
 (2) S 10°42'15" E FOR A DISTANCE OF 1838.49 FEET TO AN EXISTING ¾" IRON REBAR; THENCE
 (3) S 39°16'16" E FOR A DISTANCE OF 199.48 FEET TO AN EXISTING ½" IRON REBAR; THENCE
 (4) S 39°16'16" E FOR A DISTANCE OF 10.00 FEET TO A CALCULATED POINT ON THE TOP OF BANK OF BIG ALAMANCE CREEK;

THENCE WITH THE TOP OF BANK OF BIG ALAMANCE CREEK THE FOLLOWING SIX (6) CALLS TO CALCULATED POINTS ON THE TOP OF BANK

- (1) S 55°03'43" W FOR A DISTANCE OF 69.03 FEET; THENCE
 (2) S 79°57'14" W FOR A DISTANCE OF 67.12 FEET; THENCE
 (3) N 88°04'58" W FOR A DISTANCE OF 81.66 FEET; THENCE
 (4) S 87°26'28" W FOR A DISTANCE OF 98.46 FEET; THENCE
 (5) N 86°14'49" W FOR A DISTANCE OF 136.25 FEET; THENCE
 (6) S 75°45'24" W FOR A DISTANCE OF 103.48 FEET;

THENCE LEAVING THE TOP OF BANK OF BIG ALAMANCE CREEK ALONG THE EASTERN LINE OF THE FORKS OF ALAMANCE SUBDIVISION PROPERTY AS SHOWN ON PLAT BOOK 78, PAGE 426/427 THE FOLLOWING SEVEN (7) CALLS,

- (1) N 29°16'44" W FOR A DISTANCE OF 167.91 FEET TO A ¾" EXISTING IRON REBAR; THENCE
- (2) N 29°16'44" W FOR A DISTANCE OF 1029.54 FEET TO A ¾" EXISTING IRON REBAR; THENCE
- (3) S 76°47'52" W FOR A DISTANCE OF 400.34 FEET TO A ½" EXISTING IRON REBAR; THENCE
- (4) S 76°47'52" W FOR A DISTANCE OF 260.10 FEET TO A 1" EXISTING IRON PIPE; THENCE
- (5) N 03°19'45" W FOR A DISTANCE OF 664.75 FEET TO A ½" EXISTING IRON REBAR; THENCE
- (6) N 03°19'45" W FOR A DISTANCE OF 654.40 FEET TO AN EXISTING STONE; THENCE
- (7) N 51°51'11" W FOR A DISTANCE OF 726.37 FEET TO A ½" EXISTING IRON REBAR,

SAID REBAR BEING A COMMON CORNER WITH THE CECIL AND KRISTI EAKES PROPERTY AS DESCRIBED IN DEED BOOK 2728, PAGE 926; THENCE WITH THE EAKES PROPERTY THE FOLLOWING TWO (2) CALLS,

- (1) N 04°08'29" E FOR A DISTANCE OF 76.80 FEET TO AN IRON REBAR SET; THENCE
- (2) N 85°50'09" W FOR A DISTANCE OF 199.96 FEET TO A ¾" EXISTING IRON PIPE,

SAID IRON PIPE BEING IN THE EASTERN RIGHT OF WAY OF ROGERS ROAD, STATE ROAD # 2309; THENCE WITH THE RIGHT OF WAY OF ROGERS ROAD THE FOLLOWING FOUR (4) CALLS

- (1) ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 2896.77 FEET AND A CHORD BEARING AND DISTANCE OF N 01°20'00" E FOR A DISTANCE OF 250.87 FEET TO AN IRON REBAR SET; THENCE
- (2) N 01°19'11" W FOR A DISTANCE OF 238.26 FEET TO AN IRON REBAR SET; THENCE
- (3) N 02°09'46" W FOR A DISTANCE OF 374.54 FEET TO AN IRON REBAR SET; THENCE
- (4) N 00°14'45" W FOR A DISTANCE OF 76.39 FEET TO AN IRON REBAR SET;

THENCE LEAVING THE RIGHT OF WAY OF ROGERS ROAD ALONG THE SOUTHERN PROPERTY LINE OF THE JEAN S BONEY REVOCABLE TRUST PROPERTY AS DESCRIBED IN DEED BOOK 2519, PAGE 613 THE FOLLOWING,

S 64°05'28" E FOR A DISTANCE OF 5.36 FEET TO THE POINT AND PLACE OF BEGINNING AND CONTAINING 111.439 ACRES MORE OR LESS AS SHOWN ON A SURVEY PLAT BY GREEN MOUNTAIN ENGINEERING AND BEING GME PROJECT NUMBER 17-096.

Section 3. Notice of the public hearing shall be published once in The Alamance News, a newspaper having general circulation in the City of Graham, at least ten (10) days prior to the date of the public hearing.

Boyd Creek Pump Station:

- a. Approve Resolution Accepting Grant and Loan Assistance from The State of North Carolina for Construction of the Boyd Creek Lift Station***
- b. Approve Amendment #3 to the Boyd Creek Lift Station Project Ordinance***
- c. Award contract for the Boyd Creek Lift Station to Haren Construction Company, Inc. and authorize the Mayor, City Manager, City Attorney and City Clerk to execute the contract on behalf of the City***

Mr. Maness advised that the Boyd Creek Lift Station was originally constructed in 1959 and serves a significant portion of wastewater generated from the northwest section of the City. The age of the station, its susceptibility to flooding and the inability to find replacement parts serves as a great risk to the City's collection system and environment. He added that in August of 2017, the City was awarded \$612,500 in grant funds and \$1,837,500 in zero interest loan funds. Mr. Maness informed Council that just today, the Local Government Commission approved the City for an additional \$800,000 in zero interest loan funds to be used for this project. The City conducted two rounds of bidding with the low bid of \$2,763,000 received from Haren Construction. The proposed unit price contract is for 270 days and includes the construction of a new 700 GPM lift station, new bar screen, approximately 500 LF of 18" gravity sewer, and 350 LF of 10" force main.

Council Members and staff briefly discussed the project. Among the areas of discussion were the start date and overall length of the project, penalties for not completing the project on time, payment schedule, impact on the collection system, Haren Construction Company's reputation and awarding the unit price contract.

With no further discussion forthcoming, Mayor Peterman opened the discussion to the floor. Ms. Janice McSherry of 1047 Valley Drive Graham stepped forward to inquire about the specifics of a unit price contract. She expressed concern with escalation costs should the company hit rock. Mr. Maness advised that the City generally does have a contingency in the event a unit price contract exceeds the contract amount.

Mr. Boney stepped forward and asked what the start date would be. Mr. Maness stated that the start date would not begin until the notice to proceed has been issued. He added that happens after the pre-construction meeting has taken place and after various documents related to the project have been signed.

With no further comments forthcoming, Council Member Wiggins made a motion to approve the Resolution Accepting Grant and Loan Assistance from The State of North Carolina for Construction of the Boyd Creek Lift Station, seconded by Council Member McClure. All voted in favor of the motion.

Mayor Pro Tem Kimrey made a motion to approve Amendment #3 to the Boyd Creek Lift Station Project Ordinance, seconded by Council Member Wiggins. All voted in favor of the motion.

Council Member Wiggins made a motion to award the contract for the Boyd Creek Lift Station to Haren Construction Company, Inc. and authorize the Mayor, City Manager, City Attorney and City Clerk to execute the contract on behalf of the City, seconded by Mayor Pro Tem Kimrey. All voted in favor of the motion.

RESOLUTION ACCEPTING GRANT AND LOAN ASSISTANCE FROM THE STATE OF NORTH CAROLINA FOR CONSTRUCTION OF THE BOYD CREEK LIFT STATION

WHEREAS, The North Carolina Clean Water Revolving Loan and Grant Act of 1987 has authorized the making of loans and grants to aid eligible units of government in financing the cost of constructing wastewater treatment works, wastewater collection systems, water supply systems and water conservation projects; and

WHEREAS, The North Carolina Department of Environmental Quality previously offered a State (SRP) loan in the amount of \$1,837,500 and grant in the amount of \$612,500 from the Water Infrastructure Fund; and

WHEREAS, The City requested an additional \$800,000 in (SRP) loan funds and the North Carolina Department of Environmental Quality has now offered a State (SRP) loan in the amount of \$2,637,500; and

WHEREAS, The City of Graham intends to construct a sewer collection system project described as the replacement of the Boyd Creek Lift Station in accordance with the approved plans and specifications

NOW THEREFORE BE IT RESOLVED, BY THE CITY COUNCIL OF THE CITY OF GRAHAM:

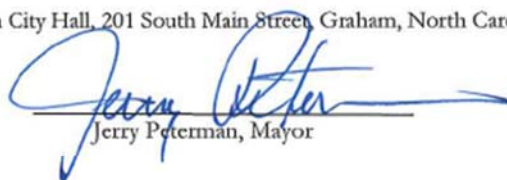
That City of Graham does hereby accept the State (SRP) loan in the total amount of \$2,637,500 and grant in the amount of \$612,500 from the Water Infrastructure Fund.

That the City of Graham does hereby give assurance to the North Carolina Department of Environmental Quality that all items specified in the loan and grant offer, Section II-Assurances will be adhered to.

That City Manager Frankie Maness, the Authorized Official, and successors so titled, is hereby authorized and directed to furnish such information as the appropriate State agency may request in connection with such application or the project; to make the assurances as contained above; and to execute such other documents as may be required in connection with the application.

That the City of Graham has substantially complied or will substantially comply with all Federal, State, and local laws, rules, regulations, and ordinances applicable to the project and to Federal and State grants and loans pertaining thereto.

Adopted this the 6th day of August, 2019 at Graham City Hall, 201 South Main Street, Graham, North Carolina.


Jerry Peterman, Mayor

ATTEST:



CERTIFICATION BY RECORDING OFFICER

The undersigned duly qualified and acting City Clerk of the City of Graham does hereby certify: That the above/attached resolution is a true and correct copy of the resolution authorizing the filing of an application with the State of North Carolina, as regularly adopted at a legally convened meeting of the City Council duly held on the 6 day of AUGUST, 2019; and, further, that such resolution has been fully recorded in the journal of proceedings and records in my office. IN WITNESS WHEREOF, I have hereunto set my hand this - 6 day of AUGUST, 2019.


Darcy Sperry, City Clerk



AMENDMENT #3 CAPITAL PROJECT ORDINANCE BOYD CREEK LIFT STATION

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAHAM, NORTH CAROLINA, that pursuant to Section 13.2, Chapter 159 of the General Statutes of North Carolina, the following Capital Project Ordinance is hereby amended:

Section 1. The Project authorized is Boyd Creek Lift Station.

Section 2. The officials of the City of Graham are hereby directed to proceed with this program within the terms of the project. Staff is authorized to execute change orders within the budget ordinance.

Section 3. The following revenues are anticipated to be available to the City to complete the project:

| | |
|--|--------------------|
| Proceeds from Water/Sewer Fund Balance | \$712,000 |
| State SRP Loan | \$2,637,500 |
| State SRP Grant | <u>\$612,500</u> |
| | \$3,962,000 |

Section 4. The following amounts are appropriated for this project:

| | |
|--------------------------------------|--------------------|
| Professional Services | \$220,000 |
| Back Creek #2 Construction | \$430,000 |
| Boyd Creek Lift Station Construction | \$3,250,000 |
| Loan Fees | <u>\$62,000</u> |
| | \$3,962,000 |

Section 5. The Finance Director shall report on the financial status of this project as directed by the City Council and will inform the Council of any unusual occurrences.

Section 6. Copies of this project ordinance shall be made available to the City Manager and the Finance Director for direction in carrying out this project.

Section 7. That this ordinance shall take effect upon passage.

This the 6th day of August, 2019.


Jerry Peterman - Mayor

ATTEST:


Darcy Sperry, City Clerk



Downtown Small Area Plan Update:

Mr. Maness presented Council with an overview of how we got here, where we go from here and what we have not seen concerning the Downtown Small Area Plan. He explained that this has been in the works for approximately five years, beginning with the Graham 2035 Comprehensive Plan. The plan balances many different issues: safety, hydrology, infill development and redevelopment, stormwater infrastructure or lack thereof, parking, commercial traffic, water & sewer infrastructure, accessibility, fire protection, aesthetics and walkability. Mr. Maness explained that last year, the NCDOT submitted their resurfacing schedule to the City. Recognizing we have aging infrastructure and other issues in the downtown area, at the City's request, NCDOT agreed to delay resurfacing project.

Mr. Maness advised that there has not yet been a plan submitted to the City. He anticipates the City will receive a draft copy of the plan sometime this month. At that time, the proposed draft plan will be reviewed and then submitted to Council for approval. Mr. Maness added that the most significant portion of the forthcoming plan is getting little if any attention. That is to provide us with an understanding of the needs surrounding our critical infrastructure that includes a rational and sequential approach to improvements that balances the issues previously mentioned. Mr. Maness stated this it is his opinion that if we lose sight of and fail to maintain critical infrastructure, nothing else matters. He stated things are more cost effective and efficient when addressed with a holistic approach. He once again reminded everyone that we have not seen a draft plan, fiscal notes, phasing plan, regulatory suggestions or requirements, nor has there been a staff critique.

Council Members spent time asking Mr. Maness about parking spaces affected by ADA compliance, sewer service in the downtown area, the cost benefit to the City by getting out ahead of NCDOT's resurfacing project and their timeline, and overall fire safety in the downtown area. Graham Fire Chief Tommy Cole and Council Members discussed flow testing requirements for the fire hydrants located downtown and the placement of fire hydrants in that area.

Mr. Boney stepped forward and expressed concern with the lack of a written report, yet the City advertised a "reveal" for the plan. He also expressed concern with the slides shown at the "reveal" differing from what was presented tonight concerning infrastructure. Mayor Peterman advised that the report is not done yet and we are giving the consultants time to submit a plan. He also reiterated that the whole reason for this plan was to slow down NCDOT's paving schedule. Mayor Pro Tem Kimrey reminded everyone that this plan was a requirement by NCDOT and could save the City of Graham hundreds of thousands of dollars by replacing old infrastructure before paving occurs. Mr. Boney asked if there is any expectation that NCDOT will be paying for any of the infrastructure improvements. Mr. Maness stated that traditionally, they do not pay for water and sewer. He did state that there might be a possibility for financial assistance with stormwater/sewer.

Mr. Chuck Talley of 808 Sideview Street Graham stepped forward and stated he does not believe there is an issue with the infrastructure in the downtown area.

Issues Not on Tonight’s Agenda:

The following individuals expressed concern with the Downtown Small Area Plan:

| | |
|--|---|
| Judy Ward – 483 W. Moore St. Graham | Ronnie James – 1619 Wedgewood Dr. Graham |
| Wendy Cates – 618 Tranquil Ct. Graham | Paul Harden – 16 NE Ct. Sq. Graham |
| Patrick Miller – 716 Trollinger Rd. Graham | Travers Webb – 619 E. Harden St. Graham |
| Janice McSherry – 1047 Valley Dr. Graham | Chuck Talley – 808 Sideview St. Graham |
| Shelly White – 111 N. Main St. Graham | Lucas Green – Petty Rd. Graham |
| Jennifer Talley – 808 Sideview St. Graham | Ben Barnhill – 404 Forest Dr. Graham |
| Jill Bullis – 500 Wildwood Ln. Graham | Roger Ausley – 300 S. Main St. Graham |
| Darlene Morgan – 10 NE Ct. Sq. Graham | Richard Shevlin – 510 Wildwood Ln. Graham |
| Tom Boney – Alamance News | Sharon Boger – 924 Hillcrest Ave. Graham |
| Nikki Smith – 402 S. Maple St. Graham | Jason Varner – 801 N. Main St. Graham |

Mrs. Talley presented Council with signed petition sheets titled: “Support our local Businesses; They need to keep their loading zones & existing pull in parking”. Additionally, she passed out a packet that included various pictures and ideas of what she would like to see.

Downtown Development Coordinator Mary Faucette updated Council on a letter recently received from the North Carolina Main Street Organization. The letter indicated that they are waiting for the budget to pass before selecting and awarding funds within the Downtown Associate Community Programs.

Council Member McClure addressed the conceptual drawings for the Small Downtown Area Plan. He stated that as a downtown business owner, parking and loading zones are important to him. He expressed concern with the phrasing used for the unveiling of the plan, stating we may have not gotten that right. He reminded everyone that what people have seen are conceptual drawings by third party consultants. Council Member McClure stated that there are pedestrian aspects of this plan that he likes, specifically mentioning condensing roadways, slowing down traffic, streetscape, walkability and encouraging people to walk from business to business.

Mayor Pro Tem Kimrey agreed with Council Member McClure’s comments about the phrasing of the plan reveal. He stated that this is a legislative process in which this Council is going to make fit our community. Pro Tem Kimrey stated that this Council is not going to do anything that will intentionally hurt business. He added that we cannot continue to grow in the downtown area without infrastructure improvements.

At 8:59 p.m., Mayor Pro Tem Kimrey made a motion to adjourn, seconded by Council Member Wiggins. All voted in favor of the motion.

Darcy Sperry, City Clerk

CITY OF GRAHAM
RELEASE ACCOUNTS

SEPTEMBER

| <u>ACCT #</u> | <u>YEAR</u> | <u>NAME</u> | <u>REASON FOR RELEASE</u> | <u>AMOUNT RELEASED</u> |
|---------------|-------------|-------------------------------|--|----------------------------|
| 29710 | 2019 | SCOGGINS, GARY DEAN | SOLD PROPERTY, RELEASE HOMESTEAD EXEMPTION | 209.50 |
| 639120 | 2019 | BARKER, THERESA G | QUALIFIED FOR HOMESTEAD EXEMPTION | 415.82 |
| 661071 | 2019 | SHORT, JOHNNY LEE REVOC TRUST | BUSINESS PP BILLED TO WRONG TAXPAYER | \$170.68 |
| 668940 | 2019 | TOMLIN, LILLY O | QUALIFIED FOR HOMESTEAD EXEMPTION | \$501.87 |
| 470487 | 2019 | PERRY, EVERETT S. JR | QUALIFIED FOR HOMESTEAD EXEMPTION | \$376.48 |
| 667244 | 2019 | HARDIE, JANET L LIFE ESTATE | QUALIFIED FOR HOMESTEAD EXEMPTION | \$201.56 |
| 674675 | 2019 | TRAILHEAD CHURCH (P# 134454) | CHURCH EXEMPTION 85% OF PROPERTY | \$1,100.56 |
| 674675 | 2019 | TRAILHEAD CHURCH (P# 134731) | PARTIAL CHURCH EXEMPTION-APPLIED LATE | \$65.60 |
| 678688 | 2019 | BUCHANAN, SILVIA | QUALIFIED FOR HOMESTEAD EXEMPTION | \$279.56 |

TOTAL RELEASES 3321.63



First United Methodist Church Graham, NC

August 22, 2019

Graham City Council Members,

First UMC Graham will be hosting a community trunk-or-treat event on Sunday, October 27 from 5pm – 6:30pm at the church (303 North Main Street). The church would like to request that East Market Street be closed from 3pm-8pm to accommodate our trick or treating area (candy give away, games, and inflatables). We plan to use only the first 50 yards of East Market Street (next to the church), so neighbors can still access their homes from the other end. We will be hiring two Graham Police officers to assist with safety and security. Please contact me via email at brian@fumcgraham-nc.org or on my cell at 919-428-1841 if you have any questions.

Thank you for considering this request!

Sincerely,

Brian H. Yoder

Brian Yoder
Director of Children and Discipleship Ministries
First United Methodist Church, Graham



CERTIFICATE OF LIABILITY INSURANCE

08/22/2019

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

| | | | |
|--|--|---|--|
| PRODUCER Church Mutual Insurance Company 3000 Schuster Lane P.O. Box 357 Merrill WI 54452 | | CONTACT NAME: Amanda R Sabatke PHONE (A/C No. Ext): 1-800-554-2642 Option 1 FAX (A/C, No): 855-264-2329 E-MAIL ADDRESS: customerservice@churchmutual.com | |
| | | INSURER(S) AFFORDING COVERAGE | |
| | | INSURER A: Church Mutual Insurance Company | |
| | | INSURER B: | |
| | | INSURER C: | |
| | | INSURER D: | |
| | | INSURER E: | |
| | | INSURER F: | |
| INSURED FIRST UNITED METHODIST CHURCH GRAHAM ALAMANCE COUNTY NORTH CAROLINA INC 303 N MAIN ST GRAHAM NC 27253-2838 | | | |

COVERAGES**CERTIFICATE NUMBER:****REVISION NUMBER:**

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

| INSR LTR | TYPE OF INSURANCE | ADDL INSD | SUBR WVD | POLICY NUMBER | POLICY EFF (MM/DD/YYYY) | POLICY EXP (MM/DD/YYYY) | LIMITS |
|----------|--|-----------|----------|-------------------|-------------------------|-------------------------|---|
| A | <input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR GEN'L AGGREGATE LIMIT APPLIES PER: <input checked="" type="checkbox"/> POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC OTHER: | Y | | 0088176-02-017925 | 08/01/2017 | 08/01/2020 | EACH OCCURRENCE \$ 2,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ 1,000,000 MED EXP (Any one person) \$ 10,000 PERSONAL & ADV INJURY \$ 2,000,000 GENERAL AGGREGATE \$ 5,000,000 PRODUCTS - COMP/OP AGG \$ 2,000,000 \$ |
| | AUTOMOBILE LIABILITY <input type="checkbox"/> ANY AUTO <input type="checkbox"/> OWNED AUTOS ONLY <input type="checkbox"/> SCHEDULED AUTOS <input type="checkbox"/> HIRED AUTOS ONLY <input type="checkbox"/> NON-OWNED AUTOS ONLY | | | | | | COMBINED SINGLE LIMIT (Ea accident) \$ BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$ \$ |
| | UMBRELLA LIAB <input type="checkbox"/> OCCUR EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE DED RETENTION \$ | | | | | | EACH OCCURRENCE \$ AGGREGATE \$ \$ |
| | WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below | Y/N | N/A | | | | <input type="checkbox"/> PER STATUTE <input type="checkbox"/> OTHER E.L. EACH ACCIDENT \$ E.L. DISEASE - EA EMPLOYEE \$ E.L. DISEASE - POLICY LIMIT \$ |

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

In compliance with contract, agreement or permit requirements, certificate holder is an additional insured under the policy. (A 267.1) SAAP565

CERTIFICATE HOLDER**CANCELLATION**

| | |
|---|--|
| CITY OF GRAHAM PO BOX 357 GRAHAM NC 272530357 | SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS. |
| | AUTHORIZED REPRESENTATIVE <i>Mandy Sabatke</i> |



STAFF REPORT

| | |
|--------------|---------------------------------------|
| SUBJECT: | ANNEXATION OF AN AREA OFF ROGERS ROAD |
| PREPARED BY: | NATHAN PAGE, PLANNING DIRECTOR |

REQUESTED ACTION:

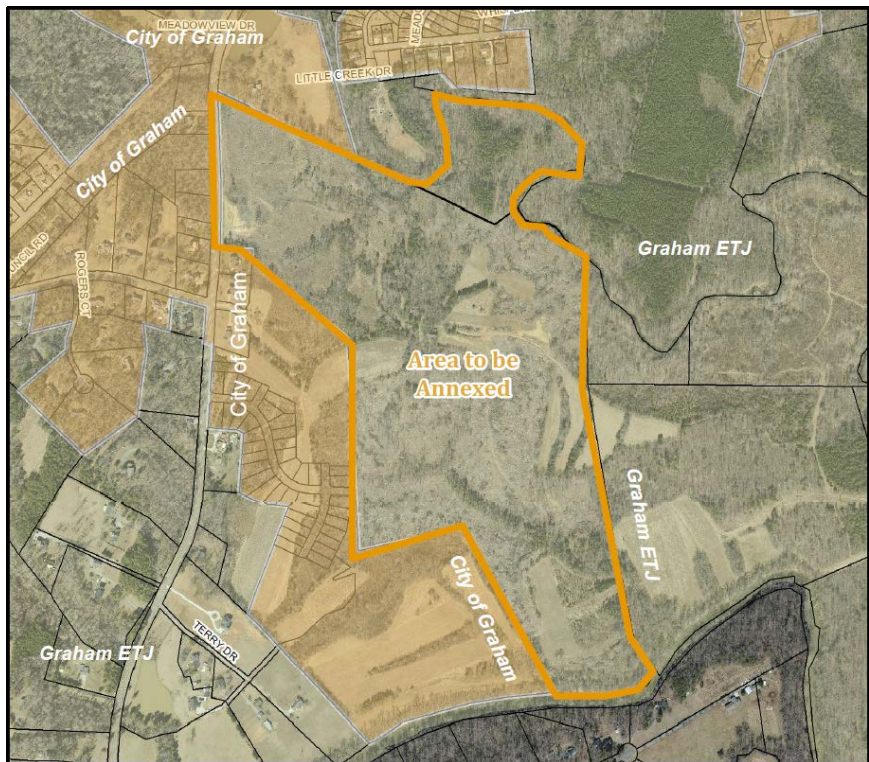
Approve the Annexation Ordinance to Extend the Corporate Limits of the City of Graham, North Carolina, for an area located off Rogers Road.

BACKGROUND/SUMMARY:

While the Council took action on a very similar item back in July of 2019, the property changed hands the day of the City Council meeting. As such, the last annexation was not legally permitted to go forward do to the applicant not being the owner of the property. This will be that same annexation which has now been properly documented as belonging to the applicant.

The attached petition seeks the Council's approval for an extension of the corporate limits to include the subject property. The area being considered for annexation is off Rogers Road (Approx 111.44). Water and Sewer are both available at this location.

The annexation process has multiple steps. Following a public hearing, approval of an Annexation Ordinance is the final step for Council in the annexation process.



FISCAL IMPACT:

The fiscal impact of this annexation to the city is unknown. While the area will have new City streets to maintain, the developer will construct them to the City's standard prior to their adoption as public.

STAFF RECOMMENDATION:

Approval. The adoption of the subject property will afford the property access to City services.

SUGGESTED MOTION(S):

I move we approve the Annexation Ordinance to Extend the Corporate limits of the City of Graham, North Carolina, for an area off Rogers Road.

ANNEXATION ORDINANCE
TO EXTEND THE CORPORATE LIMITS
OF THE
CITY OF GRAHAM, NORTH CAROLINA
FOR AN AREA OFF ROGERS ROAD (AN1901)

WHEREAS, the Graham City Council has been petitioned under G.S. 160A-31 to annex the area described below; and

WHEREAS, the Graham City Council has by resolution directed the City Clerk to investigate the sufficiency of the petition; and

WHEREAS, the City Clerk has certified the sufficiency of the petition and a public hearing on the question of this annexation was held at City Hall, 201 South Main Street, Graham at 7:00 P.M. on September 3, 2019, after due notice by publication on August 22, 2019; and

WHEREAS, the Graham City Council finds that the petition meets the requirements of G.S. 160A-31;

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Graham, North Carolina that:

Section 1. By virtue of the authority granted by G.S. 160A-31, the following described territory is hereby annexed and made part of the City of Graham as of September 30, 2019:

ALL of that certain piece, parcel or tract of land lying and being in the City of Graham, Graham Township, Alamance County, North Carolina, and being more particularly described as follows:

DESCRIPTION IS OF THE DIXIE W. ROGERS ETAL PROPERTY AS RECORDED IN DEED BOOK 550, PAGE 473 OF THE ALAMANCE COUNTY REGISTER OF DEEDS AND BEING PARCEL ID # 130476 TOGETHER WITH A PORTION OF THE SHOFFNER LAND AND TIMBER, LLC PROPERTY AS RECORDED IN DEED BOOK 2512, PAGE 695 OF THE ALAMANCE COUNTY REGISTER OF DEEDS AND BEING PARCEL ID # 131641 AND IS AS FOLLOWS:

BEGINNING AT AN EXISTING IRON PIPE, SAID PIPE HAVING A NC NAD 83 (NSRS2011) COORDINATE OF N:830,321.28, E:1,878,218.84, SAID PIPE ALSO BEING A COMMON CORNER WITH THE JEAN S BONEY REVOCABLE TRUST PROPERTY AS DESCRIBED IN DEED BOOK 2519, PAGE 613; THENCE WITH THE EASTERN LINE OF THE JEAN S BONEY REVOCABLE TRUST PROPERTY

S 64°05'28" E FOR A DISTANCE OF 835.66 FEET TO AN EXISTING 1" PINCH TOP; THENCE CONTINUING WITH THE EASTERN LINE OF THE JEAN S BONEY REVOCABLE TRUST PROPERTY S 64°16'27" E FOR A DISTANCE OF 160.00 FEET TO A CALCULATED POINT IN THE CENTERLINE OF LITTLE ALAMANCE CREEK, SAID POINT BEING A COMMON CORNER WITH THE SHOFFNER LAND AND TIMBER, LLC PROPERTY AS DESCRIBED IN DEED BOOK 2512, PAGE 695; THENCE WITH THE SHOFFNER LAND AND TIMBER, LLC. PROPERTY S 64°16'27" E FOR A DISTANCE OF 335.12 FEET TO A POINT IN THE CENTERLINE OF LITTLE ALAMANCE CREEK; THENCE THE FOLLOWING TWENTYTWO (22) CALLS TO CALCULATED POINTS IN THE CENTERLINE OF LITTLE ALAMANCE CREEK,

- (1) N 55°15'21" E FOR A DISTANCE OF 54.94 FEET; THENCE
- (2) N 85°15'23" E FOR A DISTANCE OF 148.60 FEET; THENCE
- (3) N 40°16'02" E FOR A DISTANCE OF 115.52 FEET; THENCE
- (4) N 18°10'36" W FOR A DISTANCE OF 154.72 FEET; THENCE
- (5) N 12°52'15" W FOR A DISTANCE OF 256.88 FEET; THENCE
- (6) N 34°43'03" E FOR A DISTANCE OF 75.43 FEET; THENCE WITH THE MARTIN L. SHOFFNER PROPERTY AS RECORDED IN DEED BOOK 3440, PAGE 381,
- (7) S 81°45'47" E FOR A DISTANCE OF 169.58 FEET; THENCE
- (8) S 84°04'09" E FOR A DISTANCE OF 263.67 FEET; THENCE
- (9) S 80°26'01" E FOR A DISTANCE OF 114.19 FEET; THENCE
- (10) S 66°47'27" E FOR A DISTANCE OF 190.33 FEET; THENCE

- (11) S 37°41'56" E FOR A DISTANCE OF 209.62 FEET; THENCE
- (12) S 08°21'10" E FOR A DISTANCE OF 86.52 FEET; THENCE
- (13) S 04°35'47" W FOR A DISTANCE OF 50.86 FEET; THENCE
- (14) S 43°49'02" W FOR A DISTANCE OF 38.22 FEET; THENCE
- (15) S 82°19'35" W FOR A DISTANCE OF 146.52 FEET; THENCE
- (16) S 60°50'31" W FOR A DISTANCE OF 268.25 FEET; THENCE
- (17) S 05°47'09" W FOR A DISTANCE OF 8.69 FEET; THENCE
- (18) S 05°47'09" W FOR A DISTANCE OF 38.96 FEET; THENCE
- (19) S 33°53'21" E FOR A DISTANCE OF 143.51 FEET; THENCE
- (20) S 88°11'32" E FOR A DISTANCE OF 134.20 FEET; THENCE
- (21) S 38°14'39" E FOR A DISTANCE OF 142.86 FEET; THENCE
- (22) S 49°11'57" E FOR A DISTANCE OF 191.62 FEET;

THENCE LEAVING THE CENTERLINE OF LITTLE ALAMANCE CREEK ALONG THE SOUTHERN LINE OF THE FOUST FAMILY PROPERTIES, LLC PROPERTY AS DESCRIBED IN DEED BOOK 3520, PAGE 846 S 01°38'44" W FOR A DISTANCE OF 629.73 FEET TO AN EXISTING 1 ½" IRON PIPE, SAID PIPE BEING A COMMON CORNER WITH THE ETHEL C. ROGERS HEIRS PROPERTY AS DESCRIBED IN DEED BOOK 205, PAGE 536; THENCE WITH THE ETHEL C. ROGERS HEIRS PROPERTY THE FOLLOWING FOUR (4) CALLS,

- (1) N 88°54'32" W FOR A DISTANCE OF 131.76 FEET TO AN EXISTING 1 ½" IRON PIPE; THENCE
- (2) S 10°42'15" E FOR A DISTANCE OF 1838.49 FEET TO AN EXISTING ¾" IRON REBAR; THENCE
- (3) S 39°16'16" E FOR A DISTANCE OF 199.48 FEET TO AN EXISTING ½" IRON REBAR; THENCE
- (4) S 39°16'16" E FOR A DISTANCE OF 10.00 FEET TO A CALCULATED POINT ON THE TOP OF BANK OF BIG ALAMANCE CREEK;

THENCE WITH THE TOP OF BANK OF BIG ALAMANCE CREEK THE FOLLOWING SIX (6) CALLS TO CALCULATED POINTS ON THE TOP OF BANK

- (1) S 55°03'43" W FOR A DISTANCE OF 69.03 FEET; THENCE
- (2) S 79°57'14" W FOR A DISTANCE OF 67.12 FEET; THENCE
- (3) N 88°04'58" W FOR A DISTANCE OF 81.66 FEET; THENCE
- (4) S 87°26'28" W FOR A DISTANCE OF 98.46 FEET; THENCE
- (5) N 86°14'49" W FOR A DISTANCE OF 136.25 FEET; THENCE
- (6) S 75°45'24" W FOR A DISTANCE OF 103.48 FEET;

THENCE LEAVING THE TOP OF BANK OF BIG ALAMANCE CREEK ALONG THE EASTERN LINE OF THE FORKS OF ALAMANCE SUBDIVISION PROPERTY AS SHOWN ON PLAT BOOK 78, PAGE 426/427 THE FOLLOWING SEVEN (7) CALLS,

- (1) N 29°16'44" W FOR A DISTANCE OF 167.91 FEET TO A ¾" EXISTING IRON REBAR; THENCE
- (2) N 29°16'44" W FOR A DISTANCE OF 1029.54 FEET TO A ¾" EXISTING IRON REBAR; THENCE
- (3) S 76°47'52" W FOR A DISTANCE OF 400.34 FEET TO A ½" EXISTING IRON REBAR; THENCE
- (4) S 76°47'52" W FOR A DISTANCE OF 260.10 FEET TO A 1" EXISTING IRON PIPE; THENCE
- (5) N 03°19'45" W FOR A DISTANCE OF 664.75 FEET TO A ½" EXISTING IRON REBAR; THENCE
- (6) N 03°19'45" W FOR A DISTANCE OF 654.40 FEET TO AN EXISTING STONE; THENCE
- (7) N 51°51'11" W FOR A DISTANCE OF 726.37 FEET TO A ½" EXISTING IRON REBAR,

SAID REBAR BEING A COMMON CORNER WITH THE CECIL AND KRISTI EAKES PROPERTY AS DESCRIBED IN DEED BOOK 2728, PAGE 926; THENCE WITH THE EAKES PROPERTY THE FOLLOWING TWO (2) CALLS,

- (1) N 04°08'29" E FOR A DISTANCE OF 76.80 FEET TO AN IRON REBAR SET; THENCE
- (2) N 85°50'09" W FOR A DISTANCE OF 199.96 FEET TO A ¾" EXISTING IRON PIPE,

SAID IRON PIPE BEING IN THE EASTERN RIGHT OF WAY OF ROGERS ROAD, STATE ROAD # 2309; THENCE WITH THE RIGHT OF WAY OF ROGERS ROAD THE FOLLOWING FOUR (4) CALLS

- (1) ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 2896.77 FEET AND A CHORD BEARING AND DISTANCE OF N 01°20'00" E FOR A DISTANCE OF 250.87 FEET TO AN IRON REBAR SET; THENCE
- (2) N 01°19'11" W FOR A DISTANCE OF 238.26 FEET TO AN IRON REBAR SET; THENCE
- (3) N 02°09'46" W FOR A DISTANCE OF 374.54 FEET TO AN IRON REBAR SET; THENCE
- (4) N 00°14'45" W FOR A DISTANCE OF 76.39 FEET TO AN IRON REBAR SET;

THENCE LEAVING THE RIGHT OF WAY OF ROGERS ROAD ALONG THE SOUTHERN PROPERTY LINE OF THE JEAN S BONEY REVOCABLE TRUST PROPERTY AS DESCRIBED IN DEED BOOK 2519, PAGE 613 THE FOLLOWING,

S 64°05'28" E FOR A DISTANCE OF 5.36 FEET TO THE POINT AND PLACE OF BEGINNING AND CONTAINING 111.439 ACRES MORE OR LESS AS SHOWN ON A SURVEY PLAT BY GREEN MOUNTAIN ENGINEERING AND BEING GME PROJECT NUMBER 17-096.

Section 2. Upon and after September 30, 2019, the above-described territory and its citizens and property shall be subject to all debts, laws, ordinances and regulations in force in the City of Graham and shall be entitled to the same privileges and benefits as other parts of the City of Graham. Said territory shall be subject to municipal taxes according to G.S. 160A-58.10.

Section 3. The Mayor of the City of Graham shall cause to be recorded in the office of the Register of Deeds of Alamance County, and in the office of the Secretary of State at Raleigh, North Carolina, an accurate map of the annexed territory described in Section 1 above, together with a duly certified copy of this Ordinance. Such a map shall also be delivered to the Alamance County Board of Elections, as required by G.S. 163-288.1.

Adopted this, the 3rd day of September, 2019.

Gerald R. Peterman, Mayor

ATTEST:

Darcy L. Sperry, City Clerk

APPROVED AS TO FORM:

Bryan Coleman, City Attorney

SURVEYOR'S CERTIFICATION:

I **VINCENT C. TOWNSEND**, CERTIFY THAT THIS PLAT WAS DRAWN UNDER MY SUPERVISION FROM AN ACTUAL SURVEY MADE UNDER MY SUPERVISION (DEED DESCRIPTION RECORDED IN BOOK **SEE**, PAGE **MAP**; THAT THE BOUNDARIES NOT SURVEYED ARE INDICATED AS DRAWN FROM INFORMATION IN BOOK **SEE**, PAGE **MAP**; THAT THE RATIO OF PRECISION OR POSITIONAL ACCURACY AS CALCULATED IS **1:10,000±**; AND THAT THE PLAT WAS PREPARED IN ACCORDANCE WITH G.S. 47-30 AS AMENDED.

I **VINCENT C. TOWNSEND**, CERTIFY THAT THIS IS A SURVEY OF AN EXISTING PARCEL OR PARCELS OF LAND, OR ONE OR MORE EXISTING EASEMENTS AND DOES NOT CREATE A NEW STREET OR CHANGE AN EXISTING STREET.

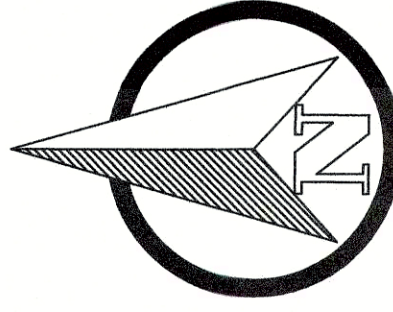
WITNESS MY ORIGINAL SIGNATURE, LICENSE NUMBER AND SEAL THIS 17 DAY OF JULY, 2019

[Signature]
PROFESSIONAL LAND SURVEYOR

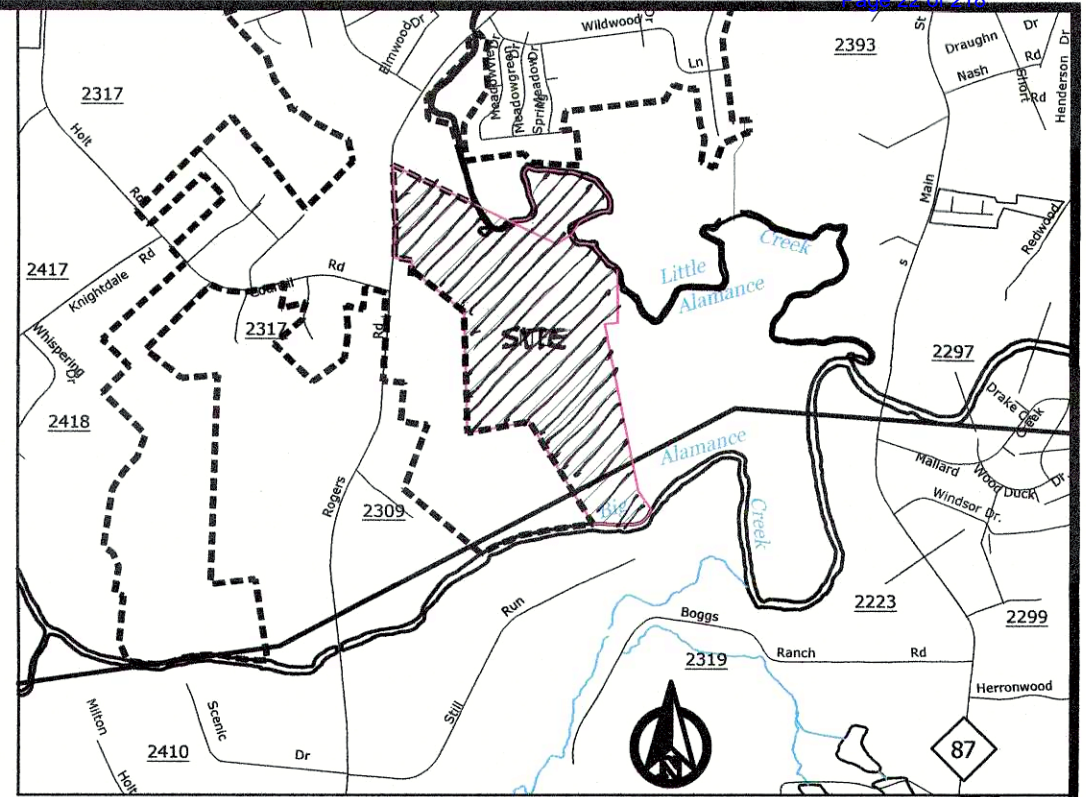


| Parcel Line Table | | |
|-------------------|--------|---------------|
| Line # | Length | Direction |
| L1 | 54.94 | N55° 15' 21"E |
| L2 | 148.60 | N85° 15' 23"E |
| L3 | 115.52 | N40° 16' 02"E |
| L4 | 154.72 | N18° 10' 36"W |
| L5 | 256.88 | N12° 52' 15"W |
| L6 | 75.43 | N34° 43' 03"E |
| L7 | 169.58 | S81° 45' 47"E |
| L8 | 263.67 | S84° 04' 09"E |
| L9 | 114.19 | S80° 26' 01"E |
| L10 | 190.33 | S66° 47' 27"E |
| L11 | 209.62 | S37° 41' 56"E |

| Parcel Line Table | | |
|-------------------|--------|---------------|
| Line # | Length | Direction |
| L12 | 86.52 | S8° 21' 10"E |
| L13 | 50.86 | S4° 35' 47"W |
| L14 | 38.22 | S43° 49' 02"W |
| L15 | 146.52 | S82° 19' 35"W |
| L16 | 268.25 | S60° 50' 31"W |
| L17 | 8.69 | S5° 47' 09"W |
| L18 | 38.96 | S5° 47' 09"W |
| L19 | 143.51 | S33° 53' 21"E |
| L20 | 134.20 | S88° 11' 32"E |
| L21 | 142.86 | S38° 14' 39"E |
| L22 | 191.62 | S49° 11' 57"E |



NC NAD83
NSRS 2011



VICINITY MAP
SCALE: 1" = 200'

CITY OF GRAHAM ANNEXATION APPROVAL

ANNEXATION APPROVED BY THE GRAHAM CITY COUNCIL ON THE _____ DAY OF _____, 2019. WITH AN EFFECTIVE DATE OF _____ PROVIDED THAT THE PLAT BE RECORDED WITHIN SIXTY DAYS OF FINAL APPROVAL.

BY _____ MAYOR _____ DATE _____
ATTEST _____ CITY CLERK _____ DATE _____

REVIEW OFFICER'S CERTIFICATE, ALAMANCE COUNTY, NORTH CAROLINA

I, _____ REVIEW OFFICER OF ALAMANCE COUNTY, CERTIFY THAT THE MAP OR PLAT TO WHICH THIS CERTIFICATION IS AFFIXED MEETS ALL STATUTORY REQUIREMENTS FOR RECORDING.

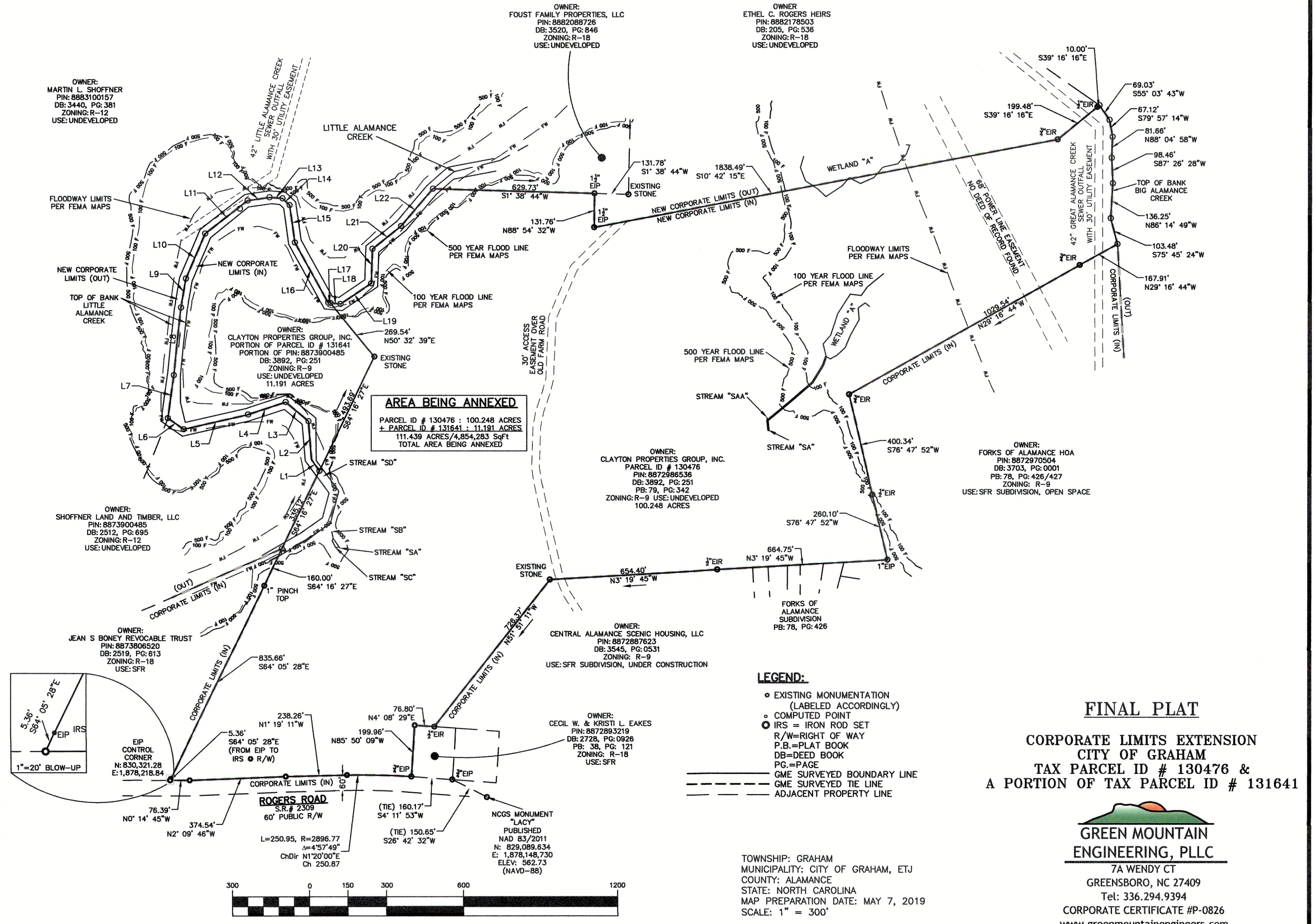
SIGNED: _____ DATE _____

PROPERTY DATA:

- DEED REFERENCE: DB 3892, PG 251, PB 79, PG 342
 - PARCEL ID # 130476, PORTION OF # 131641
 - PARCEL PIN # 8872986536, PORTION OF PIN # 8873900485
 - PROPERTY AREA: 111.439± ACRES
 - EXIST. NO. OF LOTS: 2 LOT
 - CURRENT ZONING: R-9 (GRAHAM ETJ)
 - CURRENT LAND USE: UNDEVELOPED
 - OWNER: CLAYTON PROPERTIES GROUP, INC.
5000 CLAYTON ROAD
MARYVILLE, TN 37804
- 8) NO WETLANDS PERMITTING HAS BEEN UNDERTAKEN AT THIS TIME.

SURVEY NOTES:

- THIS PROPERTY IS SUBJECT TO ANY EASEMENTS, AGREEMENTS, OR RIGHTS-OF-WAY OF RECORD PRIOR TO THE DATE OF THIS MAP WHICH WERE NOT APPARENT AT THE TIME OF OUR FIELD INSPECTION.
- THE SURVEYOR HAS NOT PERFORMED A TITLE SEARCH, THEREFORE, THIS MAP IS SUBJECT TO ANY AND ALL FACTS THAT AN ACCURATE TITLE SEARCH MAY DISCLOSE.
- NO SUBSURFACE INVESTIGATION HAS BEEN DONE AT THIS TIME. UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM ACTUAL FIELD LOCATIONS AND/OR MAPS BY OTHERS.
- ALL DISTANCES SHOWN ARE HORIZONTAL GROUND DISTANCES UNLESS OTHERWISE INDICATED.
- BEARINGS AROUND SUBJECT PROPERTY ARE ORIENTED TO THE BEARING BASE REFERENCE IN THE NORTH ARROW. ALL GRID BEARINGS ARE SO NOTED.
- IRON PINS (#4 REBAR) SET AT ALL CORNERS UNLESS OTHERWISE NOTED.
- A PORTION OF THE SUBJECT PROPERTY DOES LIE WITHIN A SPECIAL FLOOD HAZARD AREA PER FEMA MAP # 3710887200K, 3710888300K, 3710887300K, 3710888200K, 3710888300K, DATED 11/17/2017.



AREA BEING ANNEXED
PARCEL ID # 130476 : 100.248 ACRES
± PARCEL ID # 131641 : 11.191 ACRES
111.439 ACRES / 4,854,283 SQ.FT.
TOTAL AREA BEING ANNEXED

- LEGEND:**
- EXISTING MONUMENTATION (LABELED ACCORDINGLY)
 - COMPUTED POINT
 - IRS = IRON ROD SET
 - R/W=RIGHT OF WAY
 - P.B.=PLAT BOOK
 - DB=DEED BOOK
 - PG.=PAGE
 - GME SURVEYED BOUNDARY LINE
 - GME SURVEYED TIE LINE
 - ADJACENT PROPERTY LINE

FINAL PLAT
CORPORATE LIMITS EXTENSION
CITY OF GRAHAM
TAX PARCEL ID # 130476 &
A PORTION OF TAX PARCEL ID # 131641



GREEN MOUNTAIN ENGINEERING, PLLC
7A WENDY CT
GREENSBORO, NC 27409
Tel: 336.294.9394
CORPORATE CERTIFICATE #P-0826
www.greenmountainengineers.com
GME PROJECT # 17-096, ANNEXATION PLAT

TOWNSHIP: GRAHAM
MUNICIPALITY: CITY OF GRAHAM, ETJ
COUNTY: ALAMANCE
STATE: NORTH CAROLINA
MAP PREPARATION DATE: MAY 7, 2019
SCALE: 1" = 300'

| | |
|---------------------|--|
| SUBJECT: | MUTUAL AID MOU WITH ALAMANCE COUNTY |
| PREPARED BY: | BRIAN FAUCETTE, DIRECTOR OF RECREATION AND PARKS |

REQUESTED ACTION:

Authorize the City Manager to execute a Mutual Aid Memorandum of Understanding (MOU) with Alamance County for Alamance County Detention Center Evacuation.

BACKGROUND/SUMMARY:

Alamance County is requesting the approval of a MOU that seeks to provide options for the confinement of inmates from the Alamance County Detention Center in the event a disaster requires partial or complete facility evacuation. Secondly, the MOU provides an advanced understanding as it relates to expectations and responsibilities of the City and County during such an event.

The MOU stipulates the use of the Graham Recreation Center upon request and consultation with Alamance County during an event that requires evacuation. It is duly noted that the Graham Recreation Center also serves as a Red Cross Emergency Shelter and this MOU does not intend to supersede such use. The declaration of a Red Cross Emergency Shelter is currently conducted in consultation with Alamance County. Therefore, this MOU would not necessarily introduce another party.

FISCAL IMPACT:

Negligible.

STAFF RECOMMENDATION:

Approval.

SUGGESTED MOTION(S):

Authorize the City Manager to execute a Mutual Aid Memorandum of Understanding with Alamance County for Alamance County Detention Center Evacuation.

ALAMANCE COUNTY JAIL EVACUATION

Mutual Aid

Memorandum of Understanding

This Mutual Aid Memorandum of Understanding ("MOU") is dated for reference purposes only as of June 24, 2019, and is entered into voluntarily by and among Alamance County and the undersigned the City of Graham, hereinafter referred to as "Agency" or "party", that have agreed to provide mutual aid at the time of a disaster.

Nothing in this MOU is intended to create any relationship among the Agency other than that of independent entities agreeing with each other solely for the purposes set forth in this MOU.

This MOU is not legally binding on the parties, but rather is a voluntary agreement based on the belief and commitment of the undersigned Agencies that as a result of any community emergency or disaster, regardless of cause, which exceed the effective response capabilities of Alamance County, the County may request assistance from the other Agencies that are parties hereto as more generally described below.

This document is intended to (i) augment, not replace, each Agencies' disaster plan and (ii) supplement the rules and procedures governing interaction with other Agencies during a disaster. Each Agency shall have full and absolute discretion to determine the extent, if any, to which it wishes to provide resources to assist Alamance County under this MOU. Accordingly, no Agency shall be required to provide medical supplies, equipment, services, personnel or bed capacity to another Agency, either during a disaster or emergency or at any other time, regardless of available capacity or other conditions at the requesting or donating Agency. For purposes of this MOU, the disaster may be an "external" or "internal" event for one or more Agencies and is subject to an affected emergency management plan being fully implemented. The terms of this MOU are intended to be incorporated into each Agencies' emergency management plans.

By signing this MOU each Agency is evidencing its intent to abide by the terms of the MOU in the event of a disaster. The City of Graham reserves the discretion to determine the priority of use of its facilities (i.e. for use by city residents and/or Red Cross) in the event of any disaster; it is provided however that the City of Graham agrees to make reasonable efforts to comply with the following:

Term of the MOU

The term of this MOU shall be effective from June 24, 2019, through January 31, 2022. The term of this MOU shall be renewed for 3-year periods upon the terms and conditions then in effect, unless a party gives the other parties written notice of its intention not to renew, which notice shall be given no less than thirty (30) days prior to the expiration date of the then current term.

Evacuation of the Alamance County Detention Center

2.1 If a disaster affects the Alamance County Detention Center resulting in partial or complete Facility evacuation, upon request of Alamance County, the City of Graham agrees to participate in the distribution of inmates from the Alamance County Detention Center to the Graham Recreation Center. Graham Recreation Center will serve as the primary location for the Alamance County Detention Center evacuation in the event that an evacuation is warranted.

2.2 In the event of a partial or complete facility evacuation, Alamance County will contact the City Of Graham with an evacuating request per established Agency protocol, policy and guidelines.

The City Of Graham agrees to provide facility space to house at a maximum 500 inmates.

Alamance County agrees to provide the transportation of all personnel from the detention center to the Graham Recreation Center. Alamance County agrees to provide drivers for the buses to the extent possible. Alamance County will provide the necessary staff to maintain order and security on the buses during transport.

2.3 The request for the transfer of inmates by Alamance County initially may be made verbally. The request, however, must be followed with a written communication. Alamance County, to the extent possible in an emergency situation, will identify to the accepting Agency:

- * The number of inmates needing to be transferred

- * The general nature of their detention

2.4 Alamance County, to the extent possible in an emergency situation and in accordance with governing state and federal law, is responsible for providing all staff and supplies necessary to house the inmates at the evacuation site.

2.5 Alamance County as the transferring Agency is responsible for tracking the destination of all Inmates transferred out.

Supplies and Food

3.1 Alamance County by separate agreement will provide supplies and/or food for the transferred inmates during a disaster.

Transferring Inmates

4.1 The request for the "transfer" of inmates may initially be made verbally but must then be followed by written documentation of the request. An Agency is not obligated under this MOU to provide the requested personnel if the Agency does not have available personnel, or if the personnel are unwilling to provide the services under this MOU.

4.2 The "transferred" inmates will be accompanied with documentation identifying them as inmates at the check-in site of the Agency's facility.

4.3 Alamance County will reimburse the Agency lending its personnel for the actual salaries and benefits of such personnel if the personnel are employees of the Agency lending the employees. The reimbursement will be made within ninety days following receipt of an invoice.

4.4 Alamance County will be responsible for providing a mechanism for granting emergency privileges for physicians, and other licensed healthcare providers to provide services at the Agency's facility.

Miscellaneous Provisions

5.1 This MOU is governed by North Carolina law. Venue shall be proper and shall lie exclusively in the Superior Court of Alamance County North Carolina.

5.2 Amendments to this MOU must be in writing and signed by all participating Agencies.

5.3 Development of operational procedures, forms, and other tools to operationalize this MOU shall be conducted by the parties prior to any implementation of this MOU.

5.4 This MOU is in no way meant to affect any of the participating Agencies' rights, privileges, titles, claims, or defenses provided under federal or state law or common law.

5.5 The participating Agencies shall maintain the confidentiality of Inmate and other records as required by law.

Financial & Legal Liability

6.1 To the extent allowed by law and without waiving its sovereign immunity, Alamance County shall assume legal and financial responsibility for the inmates during the time the inmates are housed at the facility of the Agency. Alamance County will reimburse the donor Agency, to the extent permitted by law, for all of the Agency's costs determined by the Agency's regular rate. Costs includes all use, breakage, damage, replacement, and return costs of borrowed materials, for personal injuries that result in death, disability, loss of salary, and reasonable expenses, and for reasonable costs (including attorneys' fees and court costs) of defending any liability claims, except where the Agency may have been found to be negligent in providing preventive maintenance or proper repair of equipment which resulted in inmate injury. Reimbursement will be made within 90 days following receipt of the invoice.

6.2 Alamance County otherwise to the fullest extent permitted by law shall indemnify, defend, and hold the City of Graham harmless from and against any and all claims, liabilities, damages, injuries, losses, costs, and expenses of any kind (including attorneys' fees and court costs) incurred by the City of Graham arising from suits, claims, and/or causes of action arising from or growing out of actions or inactions on the part of Alamance County.

6.3 The Agency is responsible for appropriate credentialing of its personnel and for the safety and integrity of the equipment and supplies provided for use at the Agency.

IN WITNESS WHEREOF, we have set our hands and seals that date below written.

County of Alamance

City of Graham

County Manager, Bryan Hagood

City Manager, Frankie Maness

_____ / ____ / ____

_____ / ____ / ____

DATE

DATE

CITY OF GRAHAM
VOLUNTEER BOARDS AND COMMISSIONS
TASK FORCE REPORT
SEPTEMBER 3, 2019

The Volunteer Boards and Commissions Task Force has met four times in July and August. The group has diligently worked to envelope the full scope of the boards and commissions from their purpose to the way they conduct business, how volunteers are recruited and trained and everything in between. This is an extensive process, yet the task force has made great strides in a short period of time. Though we have accomplished much the process is not yet completed. I am anticipating that the process will take us another couple of months to complete. At that time, we will provide a nice tidy package of recommendations from the task force that have been reviewed by our attorneys.

Work that has been completed includes a listing and understanding of current boards and their roles, recommendations of the changing of a volunteer board status and any by-law recommendations that are needed as well as the recommendation for an addition to our Code of Ordinances and the Development Ordinances to address a consistency for all the boards and commissions with the exception of the ABC Board and the Housing Authority. There are numerous recommendations from this phase of work. The group is currently working on a Policy Manual for Boards and Commissions that will guide staff and board chairpersons covering everything from recruitment to training and to maintaining the volunteer boards.

The final process will be a review by the City Attorneys before the presentation is made to the Council.


GRAHAM
 NORTH CAROLINA
STAFF REPORT

| | |
|---------------------|---|
| SUBJECT: | ENCROACHMENT LICENSE AGREEMENT REQUEST BY PAUL CROTTS |
| PREPARED BY: | AARON HOLLAND, ASSISTANT CITY MANAGER |

REQUESTED ACTION:

Authorize the City manager and City Attorney to enter into an encroachment license agreement with Paul Crotts to extend the front facade at 135 S. Main Street.

BACKGROUND/SUMMARY:

Paul Crotts has submitted a written request and supporting photos to extend the front façade of his building at 135 S. Main Street. According to the letter submitted, the request is for a one-foot encroachment from the existing front wall. An encroachment license agreement approved by City Council is required pursuant to [Sec. 18-2\(c\)](#) of the City’s Code of Ordinances.



City staff has met with Mr. Crotts at the location and he has been informed that approval from NCDOT will also be required. The Historic Resources Commission granted a Certificate of Appropriateness for this property at their August 13, 2019 meeting.

FISCAL IMPACT:

There is a nominal expense to the City for costs associated with processing and monitoring of the license.

STAFF RECOMMENDATION:

Staff recommends approval. The proposed project has received all necessary reviews and approvals except from NCDOT, and there appears to be no conflict with other uses within the Right of Way.

SUGGESTED MOTION(S):

I move to authorize the City Manager and City Attorney to enter into an encroachment license agreement with Paul Crotts to extend the front façade at 135 S. Main Street as identified by the submitted photos with the following condition(s):

- Provide proof and maintenance of \$1,000,000 in liability insurance with the City as additional insured.
- Provide a final survey to confirm approved use of public space.
- Improvements under the Encroachment License must be made prior to the expiration of the building permit.



Graham
FURNITURE MART

Since 1947

**22 SW Court Square
Graham, NC 27253
336-226-6675**

August 23, 2019

Aaron Holland
Asst City Manager
PO Drawer 357
Graham, NC 27253

Re: Encroachment

Dear Aaron,

This letter is to formally request a one-foot encroachment for the front of my property located at 135 S Main St, Graham. I will forward you proof of insurance with the city listed as an additional insured. I would also like to request to be able to use the new survey that the city now has access to from Toole Design so as to not incur additional expenses.

We appreciate your full cooperation.

Kind regards,



Paul Crofts
Owner

City of Graham

P.O. Drawer 357
201 South Main Street
Graham, North Carolina 27253
336-570-6700 Fax 336-570-6703
www.cityofgraham.com

August 19, 2019

Paul Crotts
135 S. Main St.
Graham, NC 27253

RE: COA 1919 – 135 S. Main Street, Graham Furniture Exterior Facade

Mr. Crotts,

The purpose of the Historic Resources Commission is to safeguard important elements of Graham's culture, history, and architectural history to preserve those districts and landmarks for the education, pleasure and enrichment of residents and visitors. The Commission is charged with reviewing COA applications to determine if the project is in keeping with the Historic District Design Guidelines.

At its meeting on August 13, 2019, the Historic Resources Commission choose to approve COA 1919 as requested permitting the following as presented for the property located at 135 S. Main St.

- New brick face on front of building including addition of parapet wall similar to the Burke building façade as well as brick columns
- New window glass and doors
- New signs on side and front of building
- New awning

All applicable permits from the City must be obtained prior to construction. If you have any further questions please contact the City of Graham Planning Department at planner@cityofgraham.com or 336-570-6705.

Sincerely,


Nathan Page, MCRP
Planning Director
City of Graham

/ap

PLANNING ZONING BOARD

Tuesday, August 20, 2019

The Planning & Zoning Board held their regular meeting on Tuesday, August 20, 2019 in the Council Chambers of the Graham Municipal Building at 7:00 p.m. Board members present were Dean Ward, Nate Perry, Eric Crissman, Michael Benesch, Justin Moody, Bonnie Blalock and Rachel McIntyre. Staff members present were Nathan Page, Aaron Holland, and Debbie Jolly, Zoning and Inspection Technician. Vice Chair Ward called the meeting to order, gave the Overview of the Board, general meeting rules and gave the invocation.

- Dean Ward welcomed Rachel McIntyre to the board. Before going to the new business Dean Ward ask about voting on new Chair and Vice Chair. Vice Chair Ward asked for nominations. Justin Moody nominated Dean Ward seconded by Bonnie Blalock. Nate Perry nominated Eric Crissman seconded by Eric Crissman. Voting for Dean Ward as chair passed 4-3 opposed by Rachel McIntyre, Nate Perry and Eric Crissman. Voting for Nate Perry as chair failed 3-4 vote opposed by Dean Ward, Justin Moody, Bonnie Blalock and Michael Benesch. Vice Chair nomination were called for, Eric Crissman nominated Nate Perry seconded by Rachel McIntyre. Bonnie Blalock nominated Eric Crissman. Dean Ward nominated Justin Moody seconded by Bonnie Blalock. Voting for Nate Perry was 3-4 opposed by Dean Ward, Bonnie Blalock, Justin Moody and Michael Benesch. Votes for Justin Moody 4-3 opposing votes Eric Crissman, Nate Perry and Rachel McIntyre.
- Approval of the April 16, 2019 minutes. A motion was made to approve the minutes by Michael Benesch seconded by Nate Perry. All voted in favor.

a. RZ1902 Ward Street Application by Jensen Roll to rezone approximately 0.25 acres form R-12 to R-7 GPIN 8884223927. Mr. Page gave an overview of the requested rezoning. Jensen Roll explained why he wanted to rezone the property.

The following individuals spoke on this item:

| | |
|----------------|----------------|
| Anthony Pierce | 203 Ward St |
| Jensen Roll | 414 S Maple St |

Michael Benesch made a motion to approve the rezoning as it complies to the 2035 Comprehensive Plan. Seconded by Justin Moody. Ay vote Aye.

b. SU1902 Moore St Townhomes. Application by Brad Deaton-1400 Battleground Ave, Greensboro- for special Use Permit for 61 townhomes. GPIN 8883460674,8883364585 and 8883367692. Nathan Page gave a summary of the Special Use Permit. The applicant, Mr. Deaton came forward and gave his presentation.

The following individuals spoke on this item:

| | |
|--------------------|--------------------|
| Laurie Pickard | 101 Southwood Ct. |
| Bobby Minor | 315 Old Farm Dr. |
| Kim Hodge | 1128 Southwood Dr. |
| Christine Morrison | 316 Old Farm Dr. |

| | |
|------------------|--------------------|
| Steve Sumner | 414 Old Farm Rd. |
| Karen Angelichio | 1114 Southwood Dr. |
| Dana Scott | 412 old Farm Rd. |
| Scott Packard | 101 Southwood Ct. |
| Kevin Squires | 500 Old Farm Rd. |

The neighbors had concerns with traffic, parking on the street, speed limit and opening up the dead end roads. Other concerns new townhomes matching façade with what is already there, stormwater, stop signs on Moore and Old Farm. The public meeting was closed. The planning board had a discussion at this time Dean Ward mention the eight units and his concern to keep them the same as the ones that are already there. Also Mr. Ward mentioned making the 25-foot setback an undisturbed buffer, Mr. Crissman said that may be impossible when clearing land. The board discussed stormwater and other concerns. Chairman Ward opened back up public comments so board could ask Mr. Deaton some questions. Board ask about the eight units being slab construction. Mr. Deaton said he is willing to work with home owners to make changes such as façade, railing on porch, garage doors, foundation. He is willing to have a meeting with the neighbors. Mr. Sumner showed the board a video of water running through his back yard, he emailed this this Mr. Page for inclusion in the record. Chairman Ward stated he likes to give the developers time to meet with the homeowners so he would like to table this. Mr. Ward opened back up public comments for the developer. Mr. Deaton said he was fine with it being tabled. Mr. Ward closed public meeting. Michael Benesch made a motion to table until September meeting. Seconded by Justin Moody. All voted Aye.

c. RZ1903 Albany Street Duplex. Application by Cheaper Homes LLC to rezone approximately 0.25 acres from B-2 to R-7. GPIN 8874850468. Mr. Page presented the request to the Planning board. James Norman Thomas 4205 Alloway Pl, of Fayetteville stated he is the executor of the estate and stated why he wanted to sell the property. Eric made motion to deny RZ1903 based on the 2035 plan. Seconded by Justin Moody. All voted Aye. Mr. Boney ask the board to consider not denying just because the applicant is not present.

d. AM1903 Banquet Hall. Application by Aneesa Glines for a text amendment to permit banquet halls within business and industrial districts. Mr. Page gave an overview of the project. The applicant Aneesa Glines presented her project to the Planning board. Public concern with parking and zoning for current event center. After a brief discussion Eric Crissman made a motion to approve this application it consistent with the Graham 2035 Comprehensive Plan. Seconded by Nate Perry. All voted Aye.

The following individuals spoke on this item:

| | |
|-----------------|-----------------|
| Jennifer Talley | 808 Sideview St |
| Jordan Conklin | 626 Johnson Ave |
| Jan Searls | 526 E Pine St. |

Bonnie Blalock made a motion to adjourn, Nate Perry seconded. All voted Aye.

No further business the meeting was adjourned.

Respectfully Submitted,
Debbie Jolly



STAFF REPORT

Prepared by Nathan Page, Planning Director

Roll Ward Street (RZ1902)

Type of Request: Rezoning

Meeting Dates

Planning Board on August 20, 2019

City Council on September 3, 2019

Contact Information

Jensen Roll

414 S Maple St, Graham NC 27253

336-365-8673, jensenroll@gmail.com

Summary

This property is surrounded by residential properties, and across the street from the rear of the Children’s Museum. This request is to rezone the property to R-7. The stated reason for the rezoning is “Current zoning is larger than the existing lot and I would like to decrease set backs.” The property appears to be under the required 12,000 square feet for the R-12 zoning.



Location

201 Ward Street

GPIN: 8884130286

Current Zoning

Medium Density Residential (R-12)

Proposed Zoning

High Density Residential (R-7)

Overlay District

N/A

Surrounding Zoning

High Density Residential, R-7 and R-MF. Medium Density Residential, R-12, and Conditional Office and Intuitional (COI)

Surrounding Land Uses

Residential, Institutional

Size

0.25 acres

Public Water & Sewer

Connected

Floodplain

No

Staff Recommendation

Approval

Conformity to the *Graham 2035 Comprehensive Plan (GCP)* and Other Adopted Plans

Planning District: Downtown Residential

Applicable Policies;

- 2.2.1 Focused Development.** In order to maintain Graham's affordability and promote growth, the City will facilitate smart growth development by promoting infill development and focused, walkable, and mixed use built environments. *Rezoning of this parcel to R-7 would also permit duplexes here as use-by-right which could result in additional residences built within a walkable distance to the downtown.*
- 5.1.1 Housing Variety.** Encourage a mix of housing types within Graham to increase choice. These can include single family dwelling units, multifamily dwelling units, small units, pre-fabricated homes, co-housing and clustered housing. *Rezoning of this parcel to R-7 would also permit duplexes here as use-by-right.*

Development Type

Downtown Residential

3 to 6 Dwelling Units Per Acre

Principal Uses: Predominately detached single-family homes; new neighborhoods may include duplexes.

Supporting Uses: Places of worship, daycares, park facilities, schools, civic spaces, designated neighborhood centers may include neighborhood-oriented commercial, small professional offices, live-work units, and home occupations provided they do not generate excessive traffic and parking.

Applicable Strategies;

- 4.3.1 Land Use Patterns.** Promote development of efficient land use patterns to allow continues quality and efficiency of water systems. Discourage the extension of water service into areas that are not most suitable for development. *The rezoning of this parcel would allow for further density on a lot which wouldn't require additional public utilities.*
- 5.2.1 Diverse Neighborhoods.** Encourage a mix of housing types within Graham, including detached, duplex, multifamily, townhomes, and live-work units. *The proposed rezoning would allow for the construction of a Single Family Dwelling or a duplex upon the parcel.*

Staff Recommendation

Based on *The Graham 2035 Comprehensive Plan*, staff recommends **approval** of the rezoning. The following supports this recommendation:

- In accordance with Strategies 4.3.1 and 5.2.1, this rezoning will allow for an additional residence to be constructed adjacent to our walkable downtown.



Application for REZONING or CONDITIONAL REZONING

JUL 17 2019 P.O. Drawer 357
201 South Main Street
Graham, NC 27253
PLANNING DEPT. (336) 570-6705
Fax (336) 570-6703
www.cityofgraham.com

This application is for both general district rezonings and conditional rezonings. Applications are due on the 25th of each month. Applicants are encouraged to consult with the *City of Graham Development Ordinances* and the City Planner.

Site

Street Address: 201 Ward St
 Tax Map#: 145766 GPIN: 8884130286
 Current Zoning District(s):
 R-7 R-9 R-12 R-15 R-18
 R-MF R-G C-R C-MXR
 B-1 B-2 B-3 C-B C-MXC
 O-I C-O-I I-1 I-2 C-I
 Overlay District, if applicable:
 Historic S Main St/Hwy 87 E Harden St/Hwy 54
 Current Use: Single Family
 Total Site Acres: .25
 Property Owner: Jensen Roll
 Mailing Address: 414 S Maple St
 City, State, Zip: Graham, Nc 27053

Proposed Rezoning or Conditional Rezoning

Proposed Zoning District(s):
 R-7 R-9 R-12 R-15 R-18
 R-MF R-G C-R C-MXR
 B-1 B-2 B-3 C-B C-MXC
 O-I C-O-I I-1 I-2 C-I

Describe the purpose of this rezoning request. *For Conditional Rezoning*s, also specify the actual use(s) intended for the property (from Sec. 10.135 Table of Permitted Uses) along with other descriptive or pertinent information, such as number of dwelling units, type of multifamily development, square footage and number of buildings:

Current zoning is larger than the existing lot and I would like to decrease set backs.

Applicant

Property Owner Other _____

Application for Conditional Rezoning may only be initiated by the owner of a legal interest in all affected property, any person having an interest in the property by reason of written contract with owner, or an agent authorized in writing to act on the owner's behalf. If the applicant for Conditional Rezoning is other than the Property Owner, documentation in compliance with the preceding statement must be provided in order for this application to be complete.

Name: Jensen Roll
 Mailing Address: 414 S Maple St
 City, State, Zip: Graham, Nc 27053
 Phone # 336 365 8673
 Email: jenserroll@gmail.com

I have completed this application truthfully and to the best of my ability.

Jensen Roll 7/17/19
 Signature of Applicant Date

For Conditional Rezonings, this application must be accompanied by a Preliminary Site Plan and supporting information specifying the actual use(s) and any rules, regulations or conditions that, in addition to predetermined ordinance requirements, will govern the development and use of the property.

Site Plan Review Application **must be attached** to this application for Conditional Rezoning

Office Use Only. DEVID# R21902



PLANNING BOARD Recommendation & Statement of Consistency

Per NCGS 160A-383, zoning regulations shall be made in accordance with an adopted comprehensive plan and any other officially adopted plan that is applicable. The Planning Board shall advise and comment on whether the proposed amendment is consistent with "The Graham 2035 Comprehensive Plan" and any other officially adopted plan that is applicable. The Planning Board shall provide a written recommendation to the City Council that addresses plan consistency and other matters as deemed appropriate by the Planning Board, but a comment by the Planning Board that a proposed amendment is inconsistent with "The Graham 2035 Comprehensive Plan" shall not preclude consideration or approval of the proposed amendment by the City Council.

201 Ward Street (RZ1902)
Type of Request
Rezoning
Meeting Dates
Planning Board on August 20, 2019
City Council on September 3, 2019

I move to recommend **APPROVAL** of the application as presented.

I move to recommend **DENIAL**.

The application is **consistent** with *The Graham 2035 Comprehensive Plan*.

The application is **not fully consistent** with *The Graham 2035 Comprehensive Plan*.

The action is reasonable and in the public interest for the following reasons:

This report reflects the recommendation of the Planning Board, this the 20th day of August, 2019.

Attest:

Dean Ward
Dean Ward, Planning Board Vice-Chairman

Debbie Jolly
Debbie Jolly, Secretary



City Council Decision & Statement of Consistency

Per NCGS 160A-383, zoning regulations shall be made in accordance with an adopted comprehensive plan and any other officially adopted plan that is applicable. When adopting or rejecting any zoning amendment, the City Council shall also approve a statement describing whether its action is consistent with the "The Graham 2035 Comprehensive Plan" and briefly explaining why the City Council considers the action taken to be reasonable and in the public interest. The Planning Board shall provide a written recommendation to the City Council, but a comment by the Planning Board that a proposed amendment is inconsistent with the "The Graham 2035 Comprehensive Plan" shall not preclude consideration or approval of the proposed amendment by the City Council.

201 Ward Street (RZ1902)

Type of Request

Rezoning

Meeting Dates

Planning Board on August 20, 2019
City Council on September 3, 2019

Choose one...

- I move that the application be **APPROVED**.
- I move that the application be **DENIED**.

Choose one...

- The application is **consistent** with *The Graham 2035 Comprehensive Plan*.
- The application is **not fully consistent** with *The Graham 2035 Comprehensive Plan*.

State reasons...

This action is reasonable and in the public interest for the following reasons:

This report reflects the decision of the City Council, this the 3rd day of September, 2019.

Attest:

Gerald R. Peterman, Mayor

Darcy L. Sperry, City Clerk



STAFF REPORT

Prepared by Nathan Page, Planning Director

Albany Street Duplex (RZ1903)

Type of Request: Rezoning

Meeting Dates

Planning Board on August 20, 2019

City Council on September 3, 2019

Contact Information

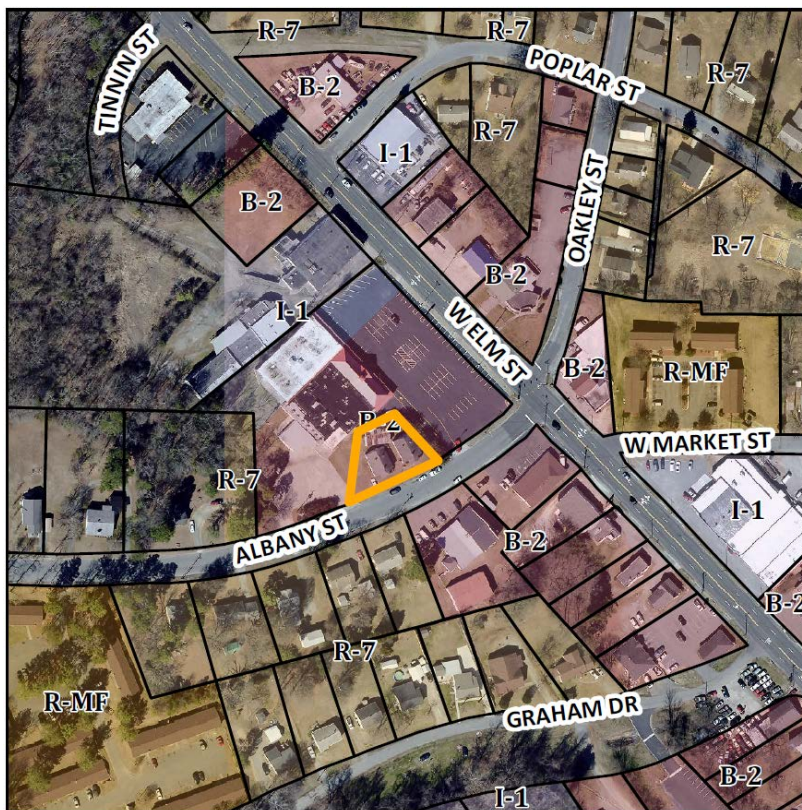
Cheaperhomes, LLC

PO Box 171, Graham NC, 27253

336-516-0489, cheaperhomes1@gmail.com

Summary

This property is surrounded by commercial properties. This request is to rezone the property to R-7. The stated reason for the rezoning is "Use lot for 2 single homes." The property appears to be under the required 11,000 square feet for two single family homes within the B-2 zoning.



Location

109 Albany Street

GPIN: 8874850468

Current Zoning

General Business (B-2)

Proposed Zoning

High Density Residential (R-7)

Overlay District

N/A

Surrounding Zoning

High Density Residential, R-7 and
General Business, B-2

Surrounding Land Uses

Residential, Commercial

Size

0.25 acres

Public Water & Sewer

Connected

Floodplain

No

Staff Recommendation

Denial

Conformity to the *Graham 2035 Comprehensive Plan (GCP)* and Other Adopted Plans

Planning District: Mixed Use Commercial

Applicable Policies;

- 2.2.1 Focused Development.** In order to maintain Graham's affordability and promote growth, the City will facilitate smart growth development by promoting infill development and focused, walkable, and mixed use built environments. *Rezoning of this parcel to R-7 would allow the two structures to be used as a single family dwelling each. However, it would reduce the total development potential of the property, and the potential for the parcel behind.*
- 5.1.1 Housing Variety.** Encourage a mix of housing types within Graham to increase choice. These can include single family dwelling units, multifamily dwelling units, small units, pre-fabricated homes, co-housing and clustered housing. *Rezoning of this parcel to R-7 would allow for two homes to be used here instead of one.*

Development Type

Mixed Use Commercial

3-5 story building heights

Principal Uses: commercial and office developments primarily located at major intersections.

Supporting Uses: Mix of retail, commercial, office, multifamily residential, and institutional uses. Buildings are multiple stories with architectural details, surface textures, and modulation of light and shade, and should be built at a human scale.

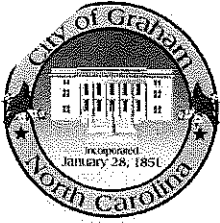
Applicable Strategies;

- 4.3.1 Land Use Patterns.** Promote development of efficient land use patterns to allow continues quality and efficiency of water systems. Discourage the extension of water service into areas that are not most suitable for development. *The rezoning of this parcel would actually be a reduction in usage, and potentially increase the cost of upgrades to the adjacent commercial parcel.*
- 5.2.1 Diverse Neighborhoods.** Encourage a mix of housing types within Graham, including detached, duplex, multifamily, townhomes, and live-work units. *The proposed rezoning would allow for the existing structures to be utilized as housing.*

Staff Recommendation

Based on *The Graham 2035 Comprehensive Plan*, staff recommends **denial** of the rezoning. The following supports this recommendation:

- In accordance with Policy 2.2.1 and Strategy 4.3.1, this rezoning will restrict the development potential of this lot, as well as negatively impact an adjacent lot.



Application for REZONING or CONDITIONAL REZONING

P.O. Drawer 357
201 South Main Street
Graham, NC 27253
(336) 570-6705
Fax (336) 570-6703
www.cityofgraham.com

This application is for both general district rezonings and conditional rezonings. Applications are due on the 25th of each month. Applicants are encouraged to consult with the *City of Graham Development Ordinances* and the City Planner.

Site

Street Address: 109 ALBANY ST.
 Tax Map#: 124-506-50 GPIN: 8874850468
 Current Zoning District(s):
 R-7 R-9 R-12 R-15 R-18
 R-MF R-G C-R C-MXR
 B-1 B-2 B-3 C-B C-MXC
 O-I C-O-I I-1 I-2 C-I
 Overlay District, if applicable:
 Historic S Main St/Hwy 87 E Harden St/Hwy 54
 Current Use: COMMERCIAL
 Total Site Acres: _____
 Property Owner: HELEN LONG HEIRS
 Mailing Address: 207 ALBANY ST
 City, State, Zip: GRAHAM NC 27253

Proposed Rezoning or Conditional Rezoning

Proposed Zoning District(s):
 R-7 R-9 R-12 R-15 R-18
 R-MF R-G C-R C-MXR
 B-1 B-2 B-3 C-B C-MXC
 O-I C-O-I I-1 I-2 C-I

Describe the purpose of this rezoning request. *For Conditional Rezoning*s, also specify the actual use(s) intended for the property (from Sec. 10.135 Table of Permitted Uses) along with other descriptive or pertinent information, such as number of dwelling units, type of multifamily development, square footage and number of buildings:

USE LOT FOR 2 SINGLE
HOMES

Applicant

Property Owner Other BUYER

Application for Conditional Rezoning may only be initiated by the owner of a legal interest in all affected property, any person having an interest in the property by reason of written contract with owner, or an agent authorized in writing to act on the owner's behalf. If the applicant for Conditional Rezoning is other than the Property Owner, documentation in compliance with the preceding statement must be provided in order for this application to be complete.

Name: Cheaperhomes LLC
 Mailing Address: PO BOX 171
 City, State, Zip: GRAHAM NC 27253
 Phone #: (336) 516-0489
 Email: C.cheaperhomes1@gmail.com

I have completed this application truthfully and to the best of my ability.

Signature of Applicant: [Signature] Date: 7-29-19

For Conditional Rezonings, this application must be accompanied by a Preliminary Site Plan and supporting information specifying the actual use(s) and any rules, regulations or conditions that, in addition to predetermined ordinance requirements, will govern the development and use of the property.

Site Plan Review Application **must be attached** to this application for Conditional Rezoning

Office Use Only. DEVID# 221903

Albany Duplex



PLANNING BOARD Recommendation & Statement of Consistency

Per NCGS 160A-383, zoning regulations shall be made in accordance with an adopted comprehensive plan and any other officially adopted plan that is applicable. The Planning Board shall advise and comment on whether the proposed amendment is consistent with "The Graham 2035 Comprehensive Plan" and any other officially adopted plan that is applicable. The Planning Board shall provide a written recommendation to the City Council that addresses plan consistency and other matters as deemed appropriate by the Planning Board, but a comment by the Planning Board that a proposed amendment is inconsistent with "The Graham 2035 Comprehensive Plan" shall not preclude consideration or approval of the proposed amendment by the City Council.

109 Albany St (RZ1903)
Type of Request
Rezoning
Meeting Dates
Planning Board on August 20, 2019
City Council on September 3, 2019

I move to **recommend APPROVAL** of the application as presented.

I move to **recommend DENIAL**.

The application is **consistent** with *The Graham 2035 Comprehensive Plan*.

The application is **not fully consistent** with *The Graham 2035 Comprehensive Plan*.

The action is reasonable and in the public interest for the following reasons:

This report reflects the recommendation of the Planning Board, this the 20th day of August, 2019.

Attest:

Dean Ward
Dean Ward, Planning Board Vice-Chairman

Debbie Jolly
Debbie Jolly, Secretary



City Council Decision & Statement of Consistency

Per NCGS 160A-383, zoning regulations shall be made in accordance with an adopted comprehensive plan and any other officially adopted plan that is applicable. When adopting or rejecting any zoning amendment, the City Council shall also approve a statement describing whether its action is consistent with the "The Graham 2035 Comprehensive Plan" and briefly explaining why the City Council considers the action taken to be reasonable and in the public interest. The Planning Board shall provide a written recommendation to the City Council, but a comment by the Planning Board that a proposed amendment is inconsistent with the "The Graham 2035 Comprehensive Plan" shall not preclude consideration or approval of the proposed amendment by the City Council.

109 Albany St (RZ1903)

Type of Request

Rezoning

Meeting Dates

Planning Board on August 20, 2019
City Council on September 3, 2019

Choose one...

- I move that the application be **APPROVED**.
- I move that the application be **DENIED**.

Choose one...

- The application is **consistent** with *The Graham 2035 Comprehensive Plan*.
- The application is **not fully consistent** with *The Graham 2035 Comprehensive Plan*.

State reasons...

This action is reasonable and in the public interest for the following reasons:

This report reflects the decision of the City Council, this the 3rd day of September, 2019.

Attest:

Gerald R. Peterman, Mayor

Darcy L. Sperry, City Clerk



STAFF REPORT

Prepared by Nathan Page, Planning Director

Text Amendment for: Section 10.135 Table of Permitted Uses to change the requirements for a Banquet Hall

Type of Request: Text Amendment

Meeting Dates

Planning Board on August 20, 2019

City Council on September 3, 2019

Contact Information

Aneesa Glines

401 W Hargett Street APT 717

Raleigh NC, 27603

aneesa@harmonyweddingsnc.com

Summary

Aneesa Glines has applied for an amendment to our Development Ordinance to permit Banquet Halls in Light Industrial.

The following amendments to the Development Ordinance are proposed:

Existing Language:

Section 10.135 Table of Permitted Uses

Project Name

Banquet Halls
(AM1903)

Location

city-wide

Current Zoning

not applicable

Proposed Zoning

not applicable

Overlay District

not applicable

Staff Recommendation

Approval

| Use Type | R-18 | R-15 | R-12 | R-9 | R-7 | C-R | R-MF | R-G | O-I | C-O-I | B-3 | B-2 | B-1 | C-B | I-1 (Note 19) | I-2 (Note 19) | C-I | C-MXR | C-MXC | LUC |
|--|------|------|------|-----|-----|-----|------|-----|-----|-------|-----|-----|-----|-----|---------------|---------------|-----|-------|-------|-----|
| Community Centers/ Banquet Hall, not including gymnasiums or stadiums | S | S | S | S | S | C | S | S | S | C | S | S | S | C | S | S | C | C | C | 3 |

Proposed Language:

Section 10.16 Definitions

Banquet Hall – An establishment which is rented by individuals or groups to accommodate private functions including, but not limited to, banquets, weddings, anniversaries and other similar celebrations. Such a use may or may not include: 1) kitchen facilities for the preparation or catering of food; 2) the sale of alcoholic beverages for on-premises consumption, only during scheduled events and not open to the general public; and 3) outdoor gardens or reception facilities.

Section 10.135 Table of Permitted Uses

| Use Type | R-18 | R-15 | R-12 | R-9 | R-7 | C-R | R-MF | R-G | O-I | C-O-I | B-3 | B-2 | B-1 | C-B | I-1 (Note 19) | I-2 (Note 19) | C-I | C-MXR | C-MXC | LUC |
|--|------|------|------|-----|-----|-----|------|-----|-----|-------|-----|-----|-----|-----|---------------|---------------|-----|-------|-------|-----|
| Community Centers not including gymnasiums or stadiums | S | S | S | S | S | C | S | S | S | C | S | S | S | C | S | S | C | C | C | 3 |
| Banquet Hall | | | | | | C | | | X | C | X | X | X | C | X | | C | C | C | 3 |

Conformity to The Graham 2035 Comprehensive Plan and Other Adopted Plans

Strategy 1.2.3 Tourism. Encourage tourism within the Town Center through promotional campaigns that highlight the Court House Square and North Main Street Historic Districts. *The removal of a Special Use Permit requirement for a Banquet Hall may result in additional tourism in our downtown.*

Planning District
All

Development Type
All

Strategy 2.1.5 Reduce Barriers. Work with local businesses and economic development partners to identify and address unnecessary barriers to local business development. *It may be that a Special Use Permit is too high a bar for Banquet Halls.*

Policy 2.3.1 Downtown. A vibrant downtown is critical for Graham’s economic success. Graham’s downtown is a priority when considering incentives, investments, regulations, and marketing. Encourage

entertainment options to locate within Graham's Downtown. *Additional event spaces could be beneficial for downtown Graham.*

Applicable Planning District Policies and Recommendations

- Not applicable; city-wide.
-

Staff Recommendation

Based on *The Graham 2035 Comprehensive Plan*, other jurisdictions and best practices, staff recommends **approval** of the text amendment. The following supports this recommendation:

- The 2035 Plan, in Strategy 1.2.3, 2.1.5, and Policy 2.3.1 recommends increasing tourism opportunities within the downtown area of Graham.



PLANNING BOARD Recommendation & Statement of Consistency

Per NCGS 160A-383, zoning regulations shall be made in accordance with an adopted comprehensive plan and any other officially adopted plan that is applicable. The Planning Board shall advise and comment on whether the proposed amendment is consistent with "The Graham 2035 Comprehensive Plan" and any other officially adopted plan that is applicable. The Planning Board shall provide a written recommendation to the City Council that addresses plan consistency and other matters as deemed appropriate by the Planning Board, but a comment by the Planning Board that a proposed amendment is inconsistent with "The Graham 2035 Comprehensive Plan" shall not preclude consideration or approval of the proposed amendment by the City Council.

Banquet Halls (AM1903)

Type of Request

Text Amendment

Meeting Dates

Planning Board on August 20, 2019

City Council on September 3, 2019

- I move to **recommend APPROVAL** of the application as presented.
- I move to **recommend APPROVAL** of the alternative language, as proposed by staff.
- I move to **recommend DENIAL**.

- The application is **consistent** with *The Graham 2035 Comprehensive Plan*.
- The application is **not fully consistent** with *The Graham 2035 Comprehensive Plan*.

The action is reasonable and in the public interest for the following reasons:

The 2035 Plan, in Strategy 1.2.3, 2.1.5, and Policy 2.3.1
recommends increasing tourism opportunities within the
downtown area of Graham

This report reflects the recommendation of the Planning Board, this the 20th day of August, 2019.

Attest:

Dean Ward

Dean Ward, Planning Board Vice-Chair

Debbie Jolly

Debbie Jolly, Secretary



City Council Decision & Statement of Consistency

Per NCGS 160A-383, zoning regulations shall be made in accordance with an adopted comprehensive plan and any other officially adopted plan that is applicable. When adopting or rejecting any zoning amendment, the City Council shall also approve a statement describing whether its action is consistent with the "The Graham 2035 Comprehensive Plan" and briefly explaining why the City Council considers the action taken to be reasonable and in the public interest. The Planning Board shall provide a written recommendation to the City Council, but a comment by the Planning Board that a proposed amendment is inconsistent with the "The Graham 2035 Comprehensive Plan" shall not preclude consideration or approval of the proposed amendment by the City Council.

Banquet Halls (AM1903)

Type of Request
Text Amendment

Meeting Dates
Planning Board on August 20, 2019
City Council on September 3, 2019

Choose one...

- I move that the text amendment be **APPROVED**.
- I move that the text amendment be **DENIED**.

Choose one...

- The text amendment is **consistent** with *The Graham 2035 Comprehensive Plan*.
- The text amendment is **not fully consistent** with *The Graham 2035 Comprehensive Plan*.

State reasons...

This action is reasonable and in the public interest for the following reasons:

This report reflects the decision of the City Council, this the 3rd day of September, 2019.

Attest:

Gerald R. Peterman, Mayor

Darcy L. Sperry, City Clerk



RECEIVED

AUG 01 2019 Application for
SPECIAL USE PERMIT
 PLANNING DEPT.

P.O. Drawer 357
 201 South Main Street
 Graham, NC 27253
 (336) 570-6705
 Fax (336) 570-6703
 www.cityofgraham.com

Uses shown as "S" in the *City of Graham Development Ordinance*, Section 10.135 Table of Permitted Uses, require a Special Use Permit before the use will be permitted in the zoning district. Applicants are strongly encouraged to consult with the City Planner to understand the requirements for the proposed special use and the information that will be needed as part of this application.

Site

Street Address: East Moore Street
 Tax Map#: 144628, 144441, 144442 GPIN: 883460674, 8883364585, 8883367692
 Current Zoning District(s): R-12
 Overlay District, if applicable:
 Historic S Main St/Hwy 87 E Harden St/Hwy 54
 Current Use: Vacant
 Property Owner: Brigham Road, LLC
 Mailing Address: 1400 Battleground Ave, Suite 201
 City, State, Zip: Greensboro, NC 27408
 Phone # (336) 274-2481
 Email: kevinp@phillipsmanagement.com

Applicant and Project Contact

Name: Bradford J. Deaton, PE
 Property Owner Other Developer
 Mailing Address: 1400 Battleground Ave, Suite 201
 City, State, Zip: Greensboro, NC 27408
 Phone # (336) 274-2481
 Email: Brad@trinterrallc.com

I certify that all information furnished is true to the best of my knowledge. I have provided all information needed to demonstrate that all requirements of the proposed special use will be met.

Bradford J. Deaton 7/31/19
 Signature of Applicant Date
Kevin Phillips 7/31/19
 Signature of Property Owner Date
 (if other than applicant)

Office Use Only. DEVID#

SUP1902

Proposed Special Use

Proposed Use (from Sec. 10.135 Table of Permitted Uses):
Townhomes

Check if this use is also listed in
 Section 10.149 Special uses listed

This application **must include sufficient information** to demonstrate that all requirements of the proposed special use will be met. Check which of the following are submitted with this application:

- Preliminary Site Plan
 Descriptive Information

Additional sheets of Descriptive Information

Because applications for a Special Use Permit go through the public hearing process, applicants are encouraged to contact neighboring property owners prior to the public hearing.

Other Requirements

- NCDOT Driveway Permit, if a new or relocated driveway is proposed on a NCDOT road, or for existing driveways if the use of the property is changing



STAFF REPORT

Prepared by Nathan Page, Planning Director

Downtown Master Plan

Type of Request:

Adoption of Downtown Master Plan

Meeting Date

City Council on September 3, 2019

Contact Information

Not applicable

Summary

The most significant portion of the proposed plan is to provide us with an understanding of the needs surrounding our critical infrastructure that includes a sequential approach to improvement that balances all of the pertinent issues to our city.

Location

Downtown and Surrounding

Staff Recommendation

Approval

The City of Graham 2035 Comprehensive Plan has 47 references to the Downtown. Just a few of the most pertinent references are included here, which the Downtown Plan will help achieve.

Strategy 1.1.1 "...and further develop plans for public improvements and landscaping in the gateway areas."

Strategy 1.1.3 "Expand public and private tree planting initiatives in appropriate locations.

Strategy 1.3.1 "Prepare Small Area Plans for Downtown..."

Policy 2.3.1 "A vibrant downtown is critical for Graham's economic success. Graham's downtown is a priority when considering incentives, investments, regulations, and marketing. Encourage entertainment options to location within Graham's downtown."

Policy 3.1.1 "New transportation infrastructure should promote and correspond with the land use goals of the Graham 2035 Comprehensive Plan, including the Future Land Use Map."

Policy 3.1.4 "To improve parking efficiency and limit the development costs of parking, promote shared parking arrangements."

Policy 3.3.2 "Reduce roadway lane widths in order to slow down vehicular traffic in areas where bicycle and pedestrian traffic is encouraged."

Policy 3.3.3 "Reduce the number of vehicular lanes in order to paint separated bicycle lanes and



allow for on-street parallel parking in areas where bicycle and pedestrian traffic is encouraged."

Policy 3.3.4 "Increase sidewalks on future and existing roads."

Policy 3.3.5 "Provide for pedestrian and bicycle safety at all street crossing, including cross-walks, bulbouts, signage, and tight curb return radii."

Strategy 3.3.3 "Amend street cross sections with the intent of increasing the right-of-way width serving pedestrians, cyclists, and low-impact stormwater infrastructure."

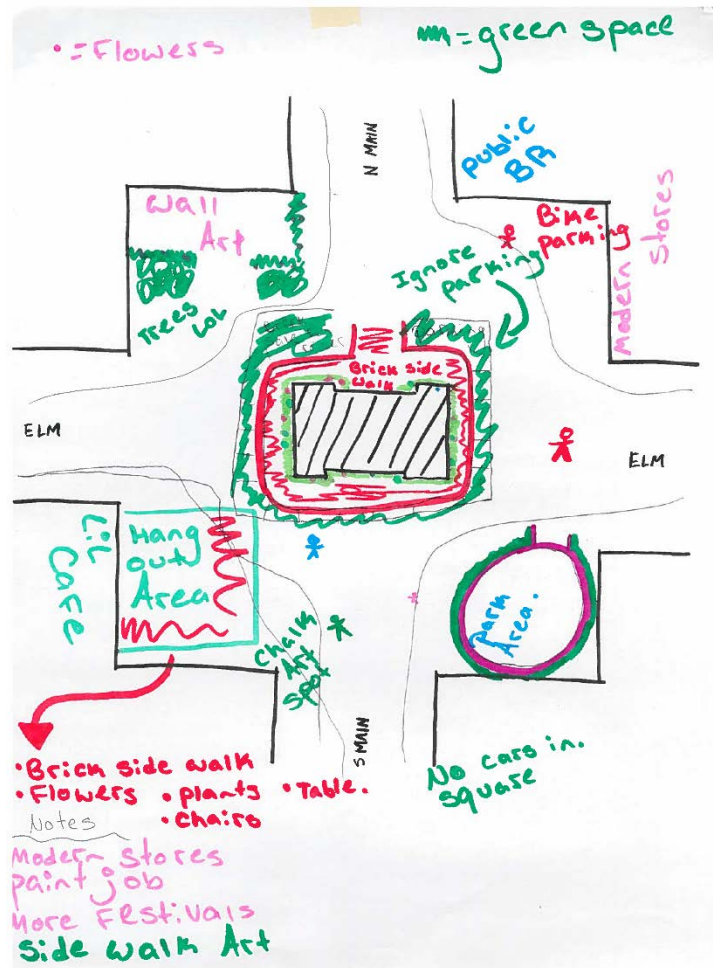
Additionally, there is a need to update much of the underground infrastructure downtown, some of which is more than 100 years old. There is an expectation that NCDOT will continue to work with the City of Graham to deal with stormwater issues for their rights of way, which if properly leveraged could result in monetary savings while also reducing the disruptions to businesses and not require the removal of freshly installed asphalt.

There are a number of issues which must be carefully considered and balanced for the plan, among them are safety, stormwater, infill and redevelopment potential, parking, commercial traffic, water and sewer infrastructure, accessibility, fire protection, aesthetics, and mobility. If the City can manage them with a **holistic approach** we can more efficiently and effectively make the required upgrades and modifications.

The City of Graham Public Works staff has identified a number of items which have been of concern for years. Amongst these are the ongoing maintenance requirements of the brick planters, as well as the difficulty of using a street sweeper to clean the corners of the roadway, particularly between the curb line and the planters. There are also concerns with regards to the crepe myrtles, both in terms of their pruning and cleaning up the litter they shed throughout the year.

Staff, upon hearing of concerns from local businesses, proposed concentrating the existing ADA spaces within the 100 block and the Courtsquare which will result in their removal from the satellite parking lots (like E Elm and Marshall) where they are underutilized. The Plan also includes flexible flush streets, which will be easier to outfit for a festival or other outdoor events downtown. The valley curb designs will permit more stormwater storage in a heavy rain event without threatening the nearby businesses or overwhelming downstream conveyance infrastructure.

Since the last downtown specific plan was completed in 1967, many things have changed. However, some pieces of that plan still ring true. "The heart, or downtown area of the city is usually its oldest part, for it is here that the early city grew and developed as a social, economic and political force. The downtown area was laid out according to the convenient means of travel of the day, which happened to be horse drawn vehicles, motor cars (in small numbers) and pedestrians. This necessitated close grouping of stores and shops." Growing Pains is also mentioned in 1967, stating concerns that are just as relevant today, "...the pains of growth caused discordant pressures from many directions which have distorted the structured community and caused it to lose much of its original identity. By this failure to



look far enough into the future, man has allowed his surroundings to change without effective controls and this could ultimately result in the orderly systems breaking down into large monotonous sprawl.”

- After learning of a plan from NCDOT to repave Highway 87 and the Courtsquare, the city initiated a planning process that would allow for the most efficient expenditure of public resources.
- The City of Graham partnered with NCDOT, the BGMPPO, and used some of our own funds to release an RFP which was advertised on September 24th of 2018.
- In October of 2018, the City of Graham received 11 submittals from different firms. Their quotes varied widely in price from \$76,000 to more than \$400,000.



- In November of 2018, the Steering Committee met with presentations and interviews of the three top-ranked consultants. Following the presentations, the Steering Committee discussed and voted Toole Design Group as their first choice consultant, with Stewart as their second choice.
- The City entered into a contract for \$150,000 with Toole Associates in January of 2019.
- Over four days in February 2019, Toole interviewed business owners, religious institutions, restaurateurs, ABSS, residents, elected officials and other stakeholders. Toole hosted an open house at Beer Co on the 26th.
- A public design workshop was held the first week of April 2019, at Trailhead’s location downtown. Throughout the week, about 60 stakeholders attended and provided input on the plan via discussion, sticky notes, and interactive polling.

- On July 11, 2019, Toole held their recommendation reveal at Trailhead's location on N Main Street. About 75 people showed up and provided input through various means including comment cards.



- In August, Toole presented the City with drafts of the plan as it evolved based upon public feedback via the elected officials. This iterative process allowed for incorporation of many of the concerns of the plans' stakeholders.

Staff Recommendation

Based on the input received throughout the comprehensive planning process, staff **recommends approval** of the Downtown Master Plan.

Suggested Motion:

I move that we adopt the Downtown Master Plan as a guide to plan for the future of the Downtown in the City of Graham.

Advisory Committee Members

Lee Kimrey
Mayor Pro Tem

Griffin McClure
City Councilmember, GABA

Cary Worthy
Historic Resources Commission

Elaine Murrin
Historic Museum Board

Carla Smith
Appearance Commission

Janee Farrar
Downtown Business Owner

Nathan Perry
Planning Board

Aaron Holland
Assistant City Manager

Nathan Page
Planning Director

Alexa Powell
Planner

Burke Robertson
Public Works Director

Mary Faucette
Downtown Development Coordinator

DOWNTOWN MASTER PLAN (DRAFT)

City of Graham, NC



ACKNOWLEDGMENTS

STAFF TEAM

Jerry Peterman, Mayor
Lee Kimrey, Mayor Pro Tem
Griffin McClure, Council Member
Chip Turner, Council Member
Melody Wiggins, Council Member
Frankie Maness, City Manager
Aaron Holland, Assistant City Manager

ADVISORY COMMITTEE

Lee Kimrey, Mayor Pro Tem
Griffin McClure, Council Member & President Of Graham Area Business Association (Gaba)
Cary Worthy, Historic Resource Commission Chair
Elaine Murrin, Historical Museum Board Chair
Carla Smith, Appearance Commission Member
Janee Farrar, Forgotten Roads Ales Owner
Aaron Holland, Assistant City Manager
Nathan Perry, Planning Board Member
Nathan Page, Planning Director
Alexa Powell, Planner
Burke Robertson, Public Works Director
Mary Faucette, Downtown Development Coordinator

PREPARED BY

Toole Design Group



Purpose of the Plan

The Graham Downtown Master Plan is a planning document that will help guide the redevelopment of Downtown Graham. This plan reviews the process of developing recommendations and illustrates concepts of how recommendations could be implemented. There are a variety of existing conditions within Downtown Graham that elevate the need for a downtown master plan, including but not limited to:

- **Aging stormwater and other underground infrastructure;**
- **North Carolina Department of Transportation's (NCDOT) scheduled repaving/resurfacing of NC 87, including Court Square;**
- **Safety for all users and the desire for more public gathering space; and**
- **Large volumes of tractor trailer trucks – specifically log trucks – in the heart of downtown.**

Each of these factors are worthy of developing a plan to move forward; however, the presence of all of them reinforces the need for an action-oriented planning document that not only provides recommendations but establishes a path forward for the City of Graham to leave a legacy for generations to come.

Updating and upsizing aging infrastructure is a critical need for Downtown Graham. To keep up with new development, redevelopment, and provide opportunities for growth, significant improvements must be considered. These improvements will require tearing up larger portions of the street in Downtown Graham and present an opportunity for a new streetscape that can increase the amount of public gathering space and promote safety for all users. The vision established throughout the Downtown Graham Master Plan will assist the City with NCDOT coordination on funding and a potential new route for trucks that will allow underground utility and above ground streetscape projects to be implemented concurrently.

It is essential that the Graham Downtown Master Plan be used as a planning resource and not as a design document. The concepts that are recommended should be further developed if and when the City decides to move forward. Details such as parking (type and number), active and passive amenities, street materials, landscaping, and many more elements described in the plan are put forth as best practice and can be refined or altered during the design phase of each project.

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DRAFT

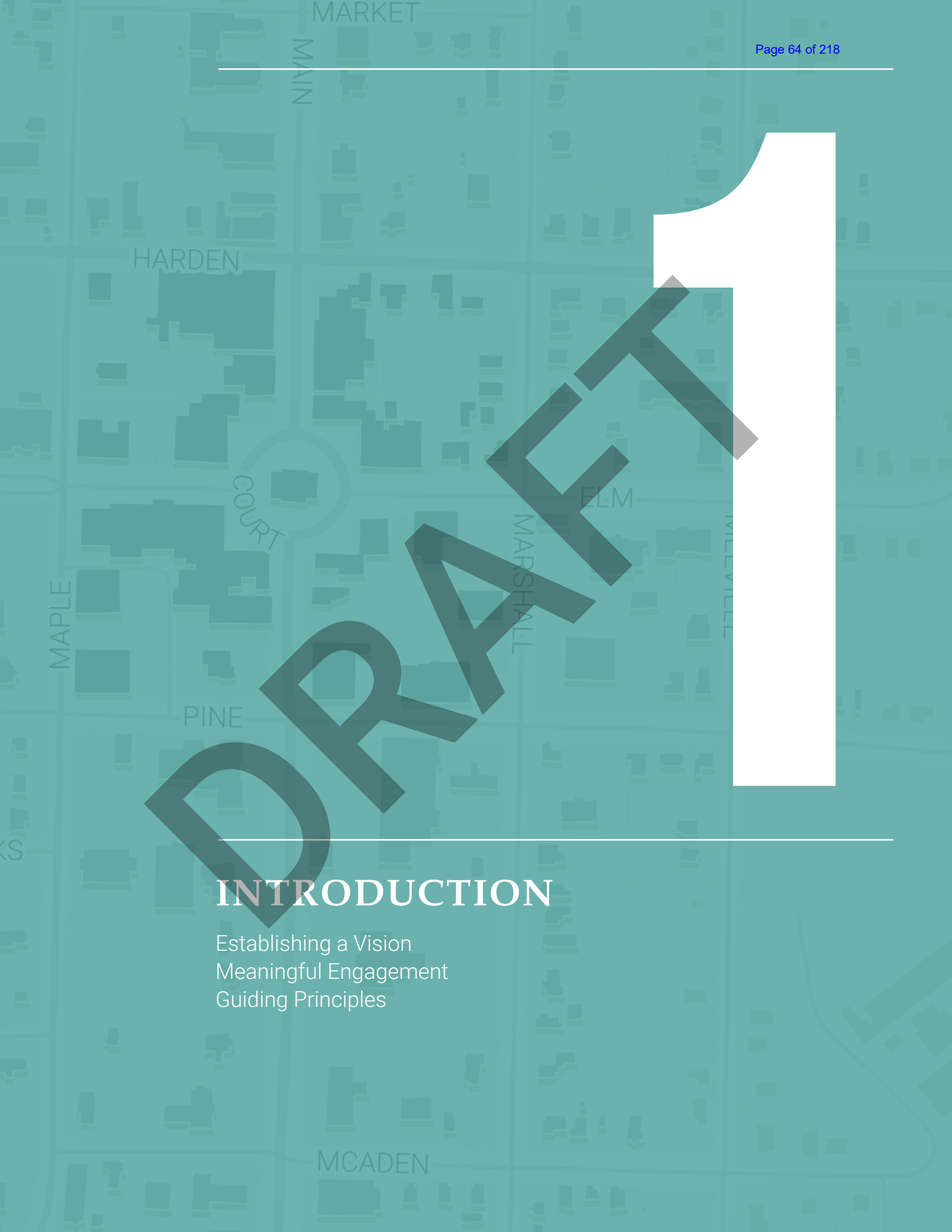
ONEIDA

ELM

MCBRIDE

HOLT

BANK



DRAFT

1

INTRODUCTION

Establishing a Vision
Meaningful Engagement
Guiding Principles

MCADEN

INTRODUCTION

The City of Graham is embarking on a journey to continue revitalizing the community through transforming downtown. Graham understands that the heart of downtown must serve several civic and commercial functions; it must be the central gathering space, center of commerce, and inviting and comfortable for residents and visitors. The City can accomplish this by focusing on the public realm – the space from building face to building face – that the City can control and influence. As public dollars are invested into the community’s public realm, investors notice and will follow suit. As projects that reimagine the public realm unfold, the fabric of the community is stitched together leaving a lasting legacy for generations.

Transformation cannot happen overnight and must start with a sound, implementable plan. The road to success takes perseverance, a steadfast staff, leadership from mayor and council, and community support standing behind the same principle foundation – to reestablish the heart of the community in downtown. This chapter seeks to introduce the City of Graham Downtown Master Plan and review how solutions have been developed. The following are key components and will be described in more detail on the following pages.

Key Components of the Plan

Establishing a Vision:

The purpose of the Graham Downtown Master Plan is to reestablish the heart of the community – Court Square and approaching 100 blocks – through reallocation of space within the public realm to better accommodate people (people who drive, walk, and bicycle within the community). Reallocation of space within the public realm will serve as an economic catalyst within Graham attracting developers from across the region.

Meaningful Engagement:

A plan of this nature must start with one simple word – LISTEN. Three workshops, stakeholder meetings, and hundreds of personal interactions allowed the community’s voice to be heard throughout the process.

Guiding Principles:

The Plan seeks to bring the community closer together. It is only fitting that our guiding principles for Graham were crafted through meaningful engagement with the community.

Establishing a Vision

The City of Graham Downtown Master Plan establishes a vision for creating a thriving destination around the Court Square and gateways into the community. This vision is focused on people. Investing in changes in downtown must consider the safety, comfort, and mobility of people of all ages and abilities.

With direct access to the Interstate 40/85 corridor and proximity to larger cities, including Greensboro, Durham, and Raleigh, a vibrant and attractive downtown can establish Graham as a regional destination where local businesses can prosper, and the community will thrive. This Plan recommends changes to Graham’s public gathering spaces, streetscaping, stormwater and utility design, traffic patterns, bicycle and pedestrian facilities, parking, and much more. While the plan focuses on Downtown Graham – including Court Square, the streets leading to and from it, and the mixture of land uses one to two blocks away from it –

a comprehensive review of the larger transportation network is also included. Recognizing that access to downtown is critical, the project team reviewed the Main Street corridor entry sequence from Interstate 40/85 to Albright Avenue, identifying opportunities for multimodal connections throughout the community. The results are recommendations that will create multimodal connections throughout Graham, transforming not only the downtown area but also the most direct artery into the heart of the community.

In developing the Plan, data collection and technical analyses were balanced with public input and comments from local stakeholders—this led to recommendations that will prepare Downtown Graham for future investments without compromising the desires of the public. While a variety of factors were considered, the following goals were paramount during the planning process and development of recommendations:

Project Goals



Safety in Design

Safety for all users can be viewed in three ways: in terms of fatalities or crashes, or in terms of user comfort.

More active transportation users and well-designed infrastructure can help reduce fatalities and crashes and increasing pedestrian and bicyclist comfort [Jacobsen, P. L. (2003). “Safety in numbers: more walkers and bicyclists, safer walking and bicycling.”].



Economic Development

Walkable neighborhoods typically have lively, populated streets and promote commercial exchanges. Having spaces that are enjoyable to walk or bicycle to can encourage more interaction at storefronts and attract more potential buyers [Litman, Todd (2018). “Economic Value of Walkability”]. Active transportation infrastructure, especially for bicycles, are powerful draws for highly-skilled employees and for younger people [Love, L. L., & Crompton, J. L. (1999). The Role of Quality of Life in Business (Re)Location Decisions.”]. Investing in active transportation now can encourage community growth a diverse industry development for years to come.



Public Health

The World Health Organization identified that inactivity is one of the leading risk factors around the world [World Health Organization (2018). “Physical Inactivity”]. People who use active transportation generally add to their daily activity by adding their travel time, and in some cases, those who commute by bike spend more time exercising in other capacities (like recreation or fitness reasons) than those who do not [Panik, Morris, Voulgaris (2019). “Does walking and bicycling more mean exercising less? Evidence from the US and the Netherlands”].

Meaningful Engagement

Listening to the community is essential to develop cohesive recommendations for Downtown Graham. Multi-day workshops allowed the project team to collect data, interview stakeholders, facilitate open house meetings, and reach out to people that live and/or work in Graham. This led to several individuals and groups offering feedback, and while comments and feedback varied, everyone's voice influenced how concepts were developed and refined.

VISIONING AND DISCOVERY WORKSHOP

In February 2019, the project team and City staff conducted a Visioning and Discovery Workshop with community stakeholders, the project advisory committee, and the general public. The workshop allowed leaders in the community to collaborate with design experts to discuss the vision for Downtown Graham. The workshop used tools such as visual preference surveys and semi-structured interviews to encourage leaders to think about what they want their community to look like in five to ten years.

The team met with several community stakeholders, including:

- **Business Owners**
- **City Staff**
- **Advisory Committee**
- **Middle School Students**
- **Members of the General Public**
- **Property Owners**
- **Local Church Leaders**
- **City Council**

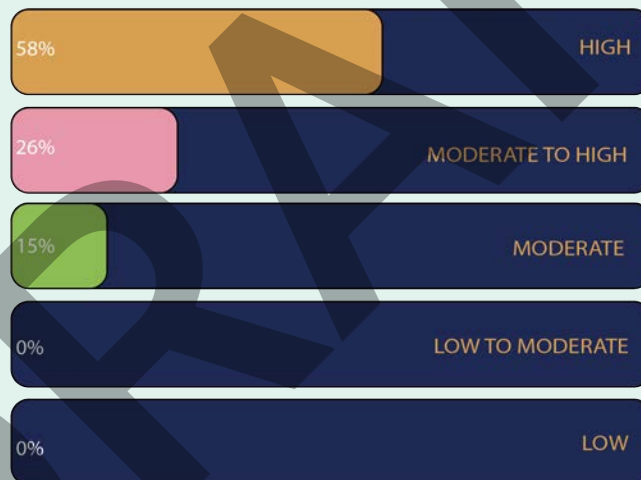
During the workshops, the project team created inventories of existing conditions, including parking counts and roadway dimensions; the team also reviewed the overall character of downtown. This data shaped the Plan's recommendations, ensuring that the vision created is both realistic and contextually sensitive.

The project team and the City hosted an Open House on February 26th at Beer Co., on West Elm Street. The event was a drop-in style meeting, with everyone in Graham invited to learn about the scope and purpose of the project and the process. Listening to the community was the main objective of this meeting. Community members were given the opportunity to express ideas, barriers, parking preferences, and development priorities through interactive activities. Participants were asked to describe their vision for Graham in a single word, and to highlight roadway segments that made them feel unsafe or uncomfortable. Findings from the Open House are illustrated in the following pages.

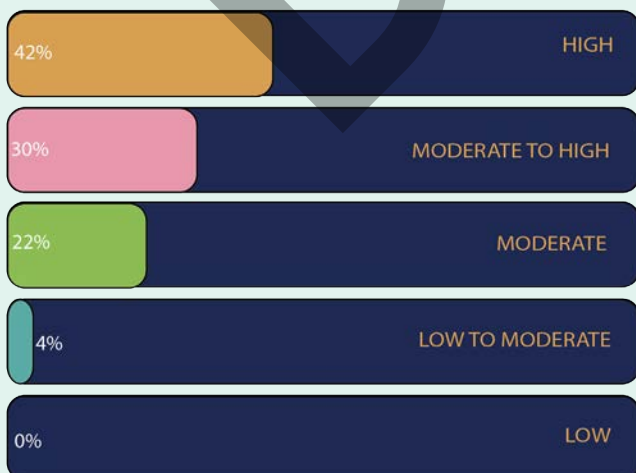
February 25th to 27th



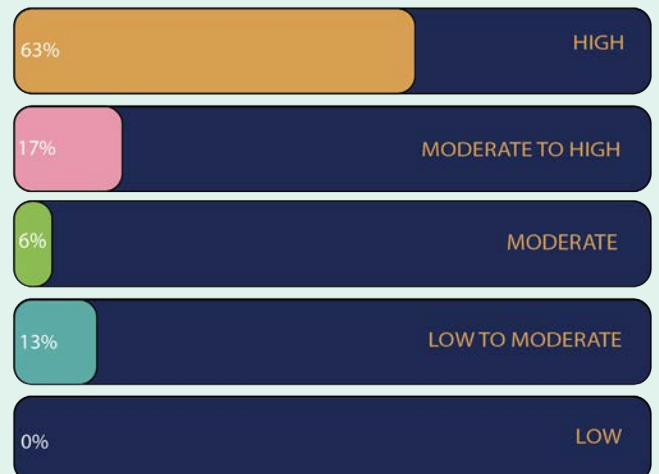
Balancing Priorities for the Local User



Balancing Priorities for the Streetscape



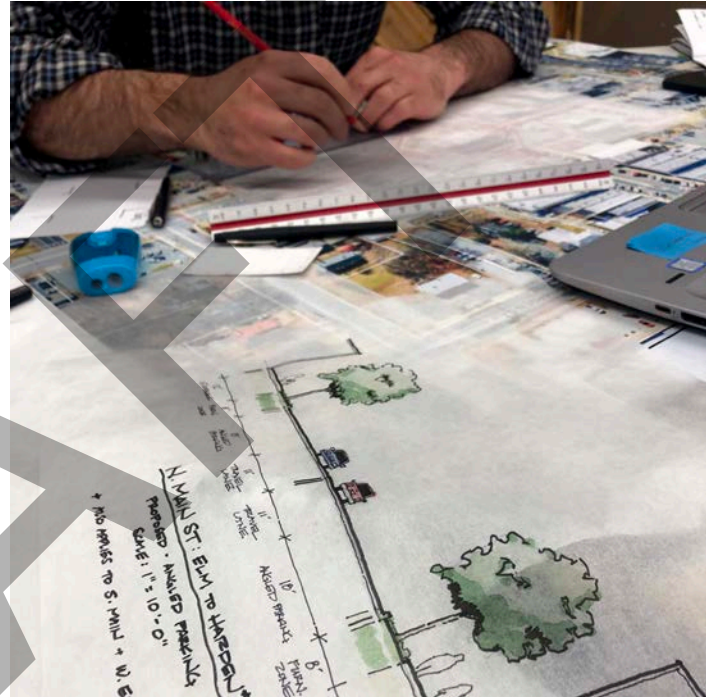
Balancing Priorities for Mobility Choice



DESIGN WORKSHOP

Following February’s Visioning and Discovery Workshop, the project team hosted a Design Workshop in early April. During this event, the team created hand-drawn concepts for downtown Graham while both answering community members’ questions about the design process and incorporating their thoughts and feedback in real time. The Design Workshop engaged several stakeholders that were a part of initial discussions during the Visioning and Discovery Workshop including middle school students from Graham Middle School.

April 1st to 4th



RECOMMENDATIONS REVEAL

Through a meaningful engagement process, the project team turned the community's desires into conceptual designs that build on Graham's character. At the start of the evening, the project team presented residents with the project's guiding principles that were derived from the two prior public workshops, design strategies that shaped the project, and conceptual plans that illustrate the potential for Graham to transform into a regional destination. At the conclusion of the presentation, an interactive open house allowed more than 80 members of the community to review and comment on the direction of the plan and have one-on-one conversations with the project team and City staff

July 11th



Guiding Principles

After listening to the community, the project team reviewed the notes, comments, and other forms of feedback to determine a set of guiding principles for developing concept designs and other recommendations for the City of Graham. The guiding principles included both themes heard from the community as well as key design principles that responded to the themes.

THEMES

Although a diverse group of community members participated in the Visioning and Discovery Workshop, it is clear that Downtown Graham is important to them all. Graham is significant to everyone in their own special way and everyone said they are excited to see what the future holds. The following are themes that capture the community's input for revitalizing downtown:

Graham Residents Want...

- To protect Downtown's historic character.
- Visitors to feel drawn to Court Square, which is the heart of downtown.
- To support small business and arts in downtown.
- Downtown to be a welcoming destination for both locals and regional visitors.
- People to feel safe downtown after dark.
- To design spaces for people of all ages.
- To retain the small-town charm.
- To build a vibrant community.

Design Principles

The themes that emerged from the public meetings and workshops shaped the City of Graham Downtown Master Plan's design principles. These principles respond to the themes expressed by the public and anchor the plan and the community to a set of common goals. Recommendations and concept design created through these design principles keep the plan focused on what Graham's residents believe is most important.

What's most important:



Intuitive access to downtown destinations

The stores, restaurants, parks, and bars in Downtown Graham are what make the area vibrant. Accessing those destinations should feel easy and natural.



Multimodal connections between places

While many places in Graham may be accessible by automobile, the recommendations in this plan will help connect places for people who are walking, biking, and wheeling.



Retain small-town aesthetics and charm

Graham's historic small-town charm gives residents a strong sense of pride and visitors a reason to make downtown a top destination.



Safety and enjoyment for all ages and abilities

From toddlers to grandparents, everyone should feel that there is something for them in Downtown Graham.



Community placemaking that brings people together

Downtown Graham should be not only a central geographic location, but also a place where the community can gather to live, work, and play.



Streets that place people first

Streets are Graham's lifblood, and they need to be designed for the people that use them.



Design for the health and wellbeing of Graham

As Downtown Graham thrives, residents will become more active and engaged with their community.

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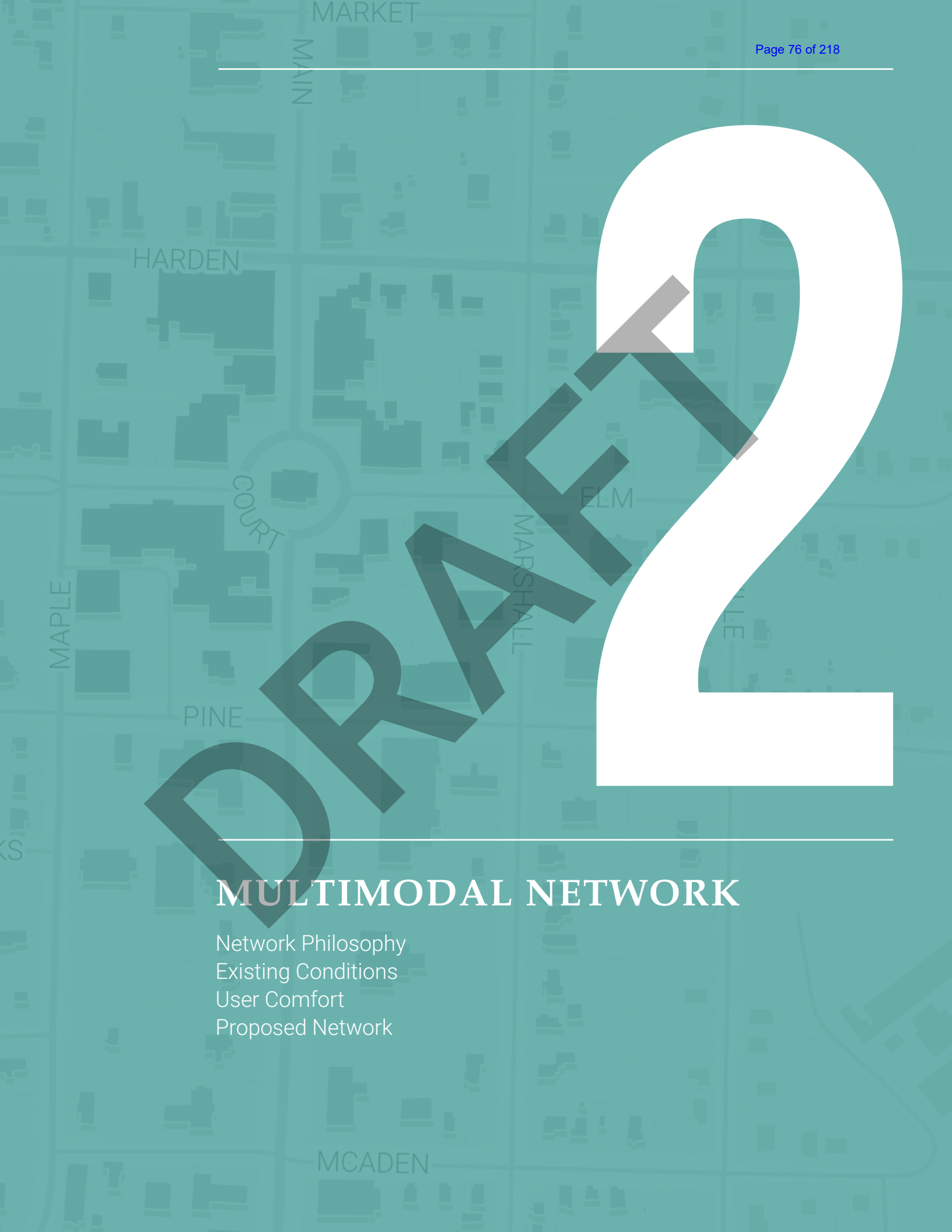
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MULTIMODAL NETWORK

- Network Philosophy
- Existing Conditions
- User Comfort
- Proposed Network



ONE WORD

USE THESE WORDS TO WRITE YOUR OWN SENTENCES

welcoming

Thriving

FOOT-TRAFFIC

THRIVING

Young Professionals

inviting
safe
happy

More Partners

to

Subsiding



MULTIMODAL NETWORK

How residents and visitors travel to and from destinations within Graham is equally important as the destination itself. The heart of Graham, situated just under a mile from Interstate 40/85, contains the intersection of two primary state routes NC 87 and NC 54. While this may draw patrons to downtown it also contributes to heavy tractor trailer, vehicle, and pedestrian conflicts - a less than ideal environment within any downtown. Residents and visitors seek mobility options to arrive at their destination that make the trip as enjoyable as the destination. Graham is fortunate to have been established on a relatively compact grid system, allowing vehicular traffic to be spread onto other roadways and short block structure for pedestrians to navigate within downtown.

Through reallocation of space along the streets of Graham, modal options can be developed that increase walkability and bikeability while improving safety for all modes of transportation within downtown. This chapter will evaluate the existing modal network and establish recommended improvements for a successful and inclusive network.

Chapter Components

Network Philosophy:

A multimodal network must be intuitive for residents and visitors, provide modal options, alleviate dependency on motor vehicles, and contribute to the overall health and wellbeing of Graham.

Existing Conditions:

In order to understand what Graham needs we must evaluate its existing modal conditions. Average vehicles per day, origins and destinations, network gaps, truck traffic, and points of interest help establish a baseline for recommendations.

User Comfort:

A network is only as good as its perceived comfort. Is design self-enforcing of speed? Are sidewalks wide enough and separated by street trees and/or on-street parking? Are bicycle facilities appropriate for people of all ages and abilities and designed for the adjacent roadway speed? The answers to these questions and many more dictate the basic principles that must be established to facilitate user comfort for all future public and private projects.

Proposed Network:

The proposed network is a community wide connectivity plan that establishes Graham as a connected, engaged, and active community.

Network Philosophy

The Graham Downtown Master Plan’s recommendations largely address Court Square in Downtown Graham. While vehicle movement is an important aspect for all communities, a connected transportation network can not be complete without modal options to reach a desired destination. These are listed first among all recommendations because the concept designs were shaped by the desire to restore Court Square’s original intent - to prioritize commerce and people within Downtown Graham.

As Downtown Graham invests in the public realm, becoming a more inviting place for residents and visitors, the Court Square and connected network of streets must provide safe, comfortable and intuitive infrastructure for people to walk, bike, and drive. The recommendations in the City of Graham Downtown Master Plan are formed to satisfy today’s demand for a more vibrant community in a way that still accommodates its long-term potential. Once implemented, recommendations from this plan will encourage more trips to Downtown Graham. These trips can be completed on a variety of transportation modes, each providing appropriate infrastructure that emphasizes that downtown is a place to go to and not merely travel through.

WHAT MAKES A GOOD MULTIMODAL NETWORK?

A well-designed multimodal network must consider and address the safety and comfort of all users while providing appropriate access to destinations within a community. The transportation system should provide mobility options and accommodate and/or prioritize more vulnerable users such as pedestrians and people riding bicycles along travel corridors and intersections. Safety should be prioritized over saving a few seconds per trip and the downtown multimodal network should be a place where all users are welcome. While the street network is connected for motorized vehicles, other modes of travel have fragmented or limited infrastructure. While installing a single bicycle or pedestrian facility might support and encourage active travel in one specific place, it is unlikely to have a greater impact on how community members travel. The more effective approach to encouraging bicycling and walking is through developing a network of interconnected facilities that run between key destinations.

Complete networks promote the following elements for all modes of transportation with specific attention to the needs of pedestrians and people on bicycles:



SAFETY

The frequency and severity of crashes are minimized and conflicts with motor vehicles are limited.



COMFORT

Conditions do not deter bicycles due to stress, anxiety, or concerns over safety.



CONNECTIVITY

All destinations can be accessed using a complete bicycle and pedestrian network with no gaps or missing links.



DIRECTNESS

Bicycling distances and trip times are minimized.



COHESION

Distances between parallel and intersecting bike routes are minimized.



ATTRACTIVENESS

Routes direct bicyclists and pedestrians through lively areas and personal safety is prioritized.



UNBROKEN FLOW

Stops, such as long waits at traffic lights, are limited and street lighting is consistent.

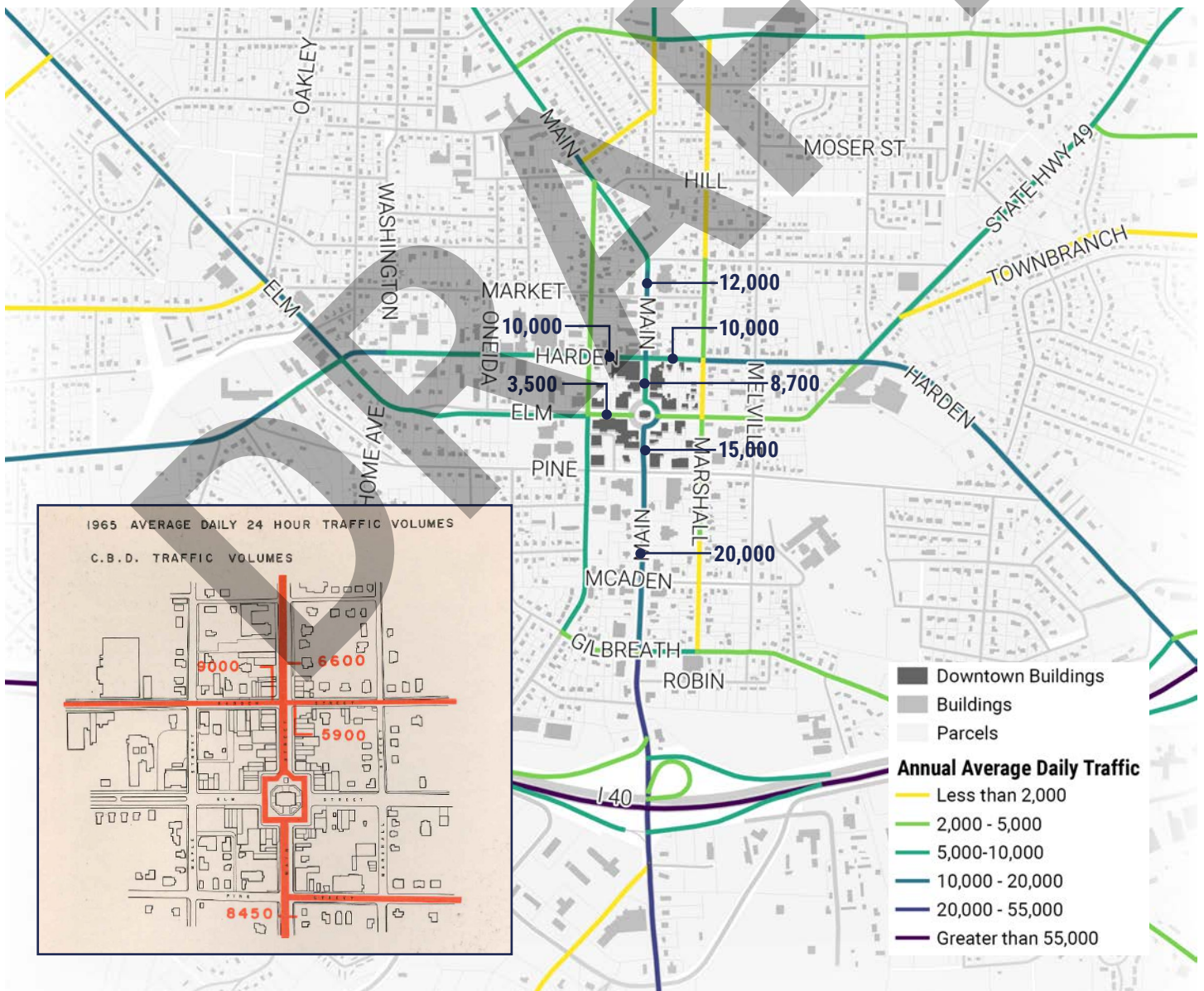
Existing Conditions

The project team reviewed the existing conditions throughout the community to understand the overall circulation and movement of people. Downtown Graham includes several short and walkable street blocks surrounding the Court Square. While some infrastructure exists (i.e., sidewalks, crosswalks, etc.) that would allow people to move to and around downtown Graham, most of the street network is designed to move vehicles and reduce delay. Decisions made in the past to designate Main Street as NC 87 continues to bring people to Graham but has limited the ability to reclaim the street as public space for all users. Several of the streets in and approaching Downtown Graham are lacking sidewalks on one or both sides. At

present, there are no bicycle facilities within a quarter mile of Downtown Graham. A bike lane is planned for Pine Street that will provide connection east and west across the community.

Traffic volumes across the community have remained consistent for several years. In fact, volumes are similar to counts recorded in 1965 as seen below. Although there are a large number of tractor trailer trucks that use NC 87 to travel north and south of Interstate 40/85, the network of parallel routes has the capacity to accommodate additional vehicular traffic. The following map illustrates the traffic counts on the street network in and around Downtown Graham.

City of Graham Traffic Volumes



SIDEWALK INVENTORY

Sidewalks provide basic mobility for people walking and wheeling. In order for people to feel comfortable walking and wheeling for trips, they must be separated from fast-moving traffic from the beginning to the end of their trip. Disconnects in a sidewalk network reduce safety by requiring people to walk in the street to reach their destination, and limiting accessibility for those with physical disabilities. The map below shows the existing sidewalks and gaps along much of the transportation system in the City of Graham.

Proposed Elm Street Cross Section



User Comfort

A network is only as good as its perceived comfort. Is design self-enforcing of speed? Are sidewalks wide enough and separated by street trees and/or on-street parking? Are bicycle facilities appropriate for people of all ages and abilities and designed for the adjacent roadway speed? The answers to these questions and many more dictate the basic principles that must be established to facilitate user comfort for all future public and private projects.

Reimagining Downtown Graham starts with the comfort of all users. As the project team listened to the public through the Vision and Discovery Workshop, it was clear that although aesthetics, parking, and design character for downtown are important, the desire for a comfortable and safe infrastructure network to travel to and from downtown is critical. When considering new design concepts for Court Square and corridors approaching Downtown Graham, the project team focused on prioritizing safety for all users and incorporating gathering spaces for people, while designing for 20 mph traffic through Downtown Graham.

Pedestrian comfort is paramount for the City of Graham Downtown Master Plan. Downtown is accessible for several nearby neighborhoods as well as Graham Middle School. Separation from vehicular traffic is prioritized to promote safety but also provide opportunity for streetscape enhancements along several corridors. Increasing space for people to gather and streetscapes recommendations are fully discussed for Court Square, Elm Street, and Main Street in Chapter 3.

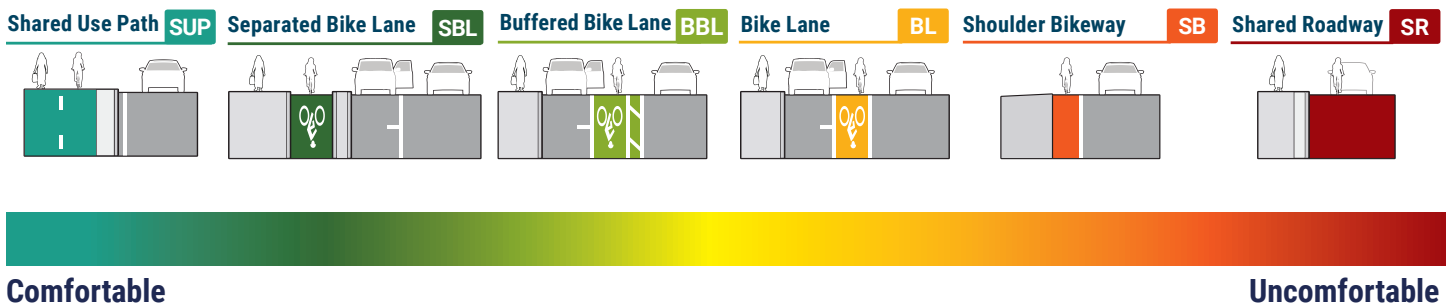
The project team assessed the existing network to identify connections for a network of bikeway facilities to connect Downtown Graham with other nearby destinations and/or residential areas. Bikeway facility recommendations consider specific criteria that impact the comfort of bicycle users. Designating appropriate facilities for people to travel on bicycles provides another mobility option for residents and visitors to explore Downtown Graham.

Bicyclist Comfort

Many people in the United States may not feel comfortable bicycling in mixed traffic scenarios; they may feel like traffic moves too fast, they may be less confident in their abilities, or they may be traveling with children or less-experienced family members and do not feel that bicycling is safe for the people joining them. The most effective networks—i.e., those that serve the most people—cater to the needs of those who make up this majority. Studies have found correlations between a user’s perceived safety and their actual safety on the roadway.

To make these potential users feel safer, bicycle facilities must be separated from vehicular travel by horizontal distance and/or vertical separations. Horizontal distance moves the bicyclists away from motorized traffic into travel space that is delineated for them, and vertical separation creates visual and physical barriers between faster motorized travel and bicyclists.

Bicyclist Comfort According to Facility Type



Proposed Network

This Plan's recommendations for a multimodal network were based on the goals, existing conditions, guiding principles listed in Chapter 1, and best design practices from successful installations across the United States. A variety of recommendations were developed for the overall network including:

Recommendations

Gateways

Key intersections that serve as an entry into Downtown Graham. Although some of these intersections are proposed outside of the core of what many consider downtown, they are locations that should signify a transition into a more downtown environment where people move in a variety of modes and vehicular traffic speeds are low;

Gateway Corridors

Streets that bring residents and visitors directly into the downtown environment. Not all gateway corridors begin/end at the aforementioned Gateways, but each corridor connects the community directly to the Court Square;

Secondary Routes

Connecting and parallel streets that allow for travelers in Graham to go around downtown but also provide access to a variety of other businesses and residences; and

Bicycle and Pedestrian Facilities

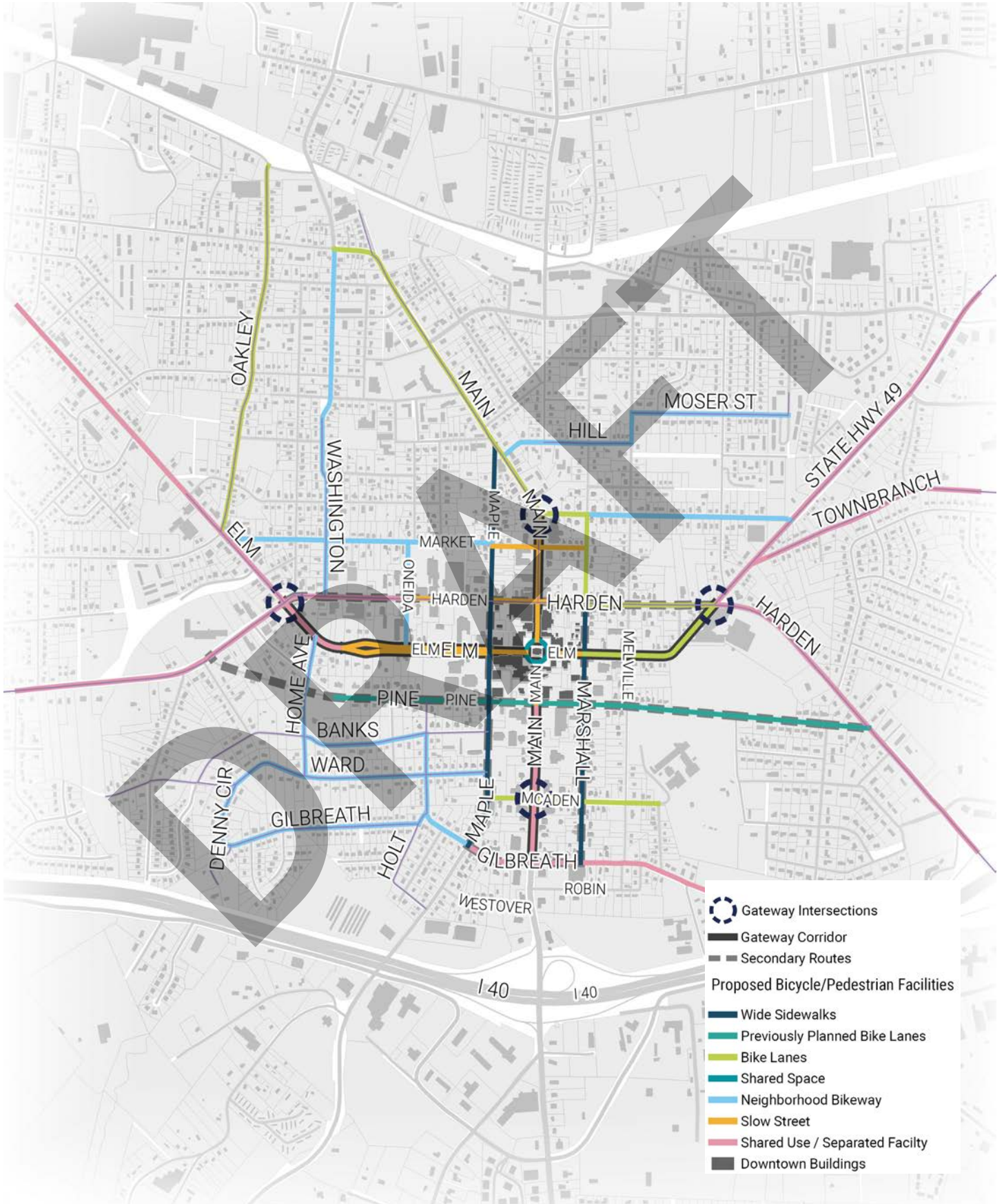
A network of proposed facilities that provide designated space for bicycle users and pedestrians to not only access Downtown Graham but to explore the entire community.

GATEWAYS AND SECONDARY ROUTES

The map on the following page illustrates the proposed network of Gateway Corridors and Secondary Routes along with locations for Gateway features and proposed street segments for reallocating existing right-of-way to provide new street cross sections.

NCDOT coordination is a critical component of these recommendations. The City should discuss the opportunity to re-route NC 87 off Main Street to remove heavy truck traffic in Downtown Graham. The City should also discuss the opportunity to take over the ownership of Main Street from NCDOT through a road swap of Marshall Street.

Proposed Network: Gateway Corridors and Secondary Routes



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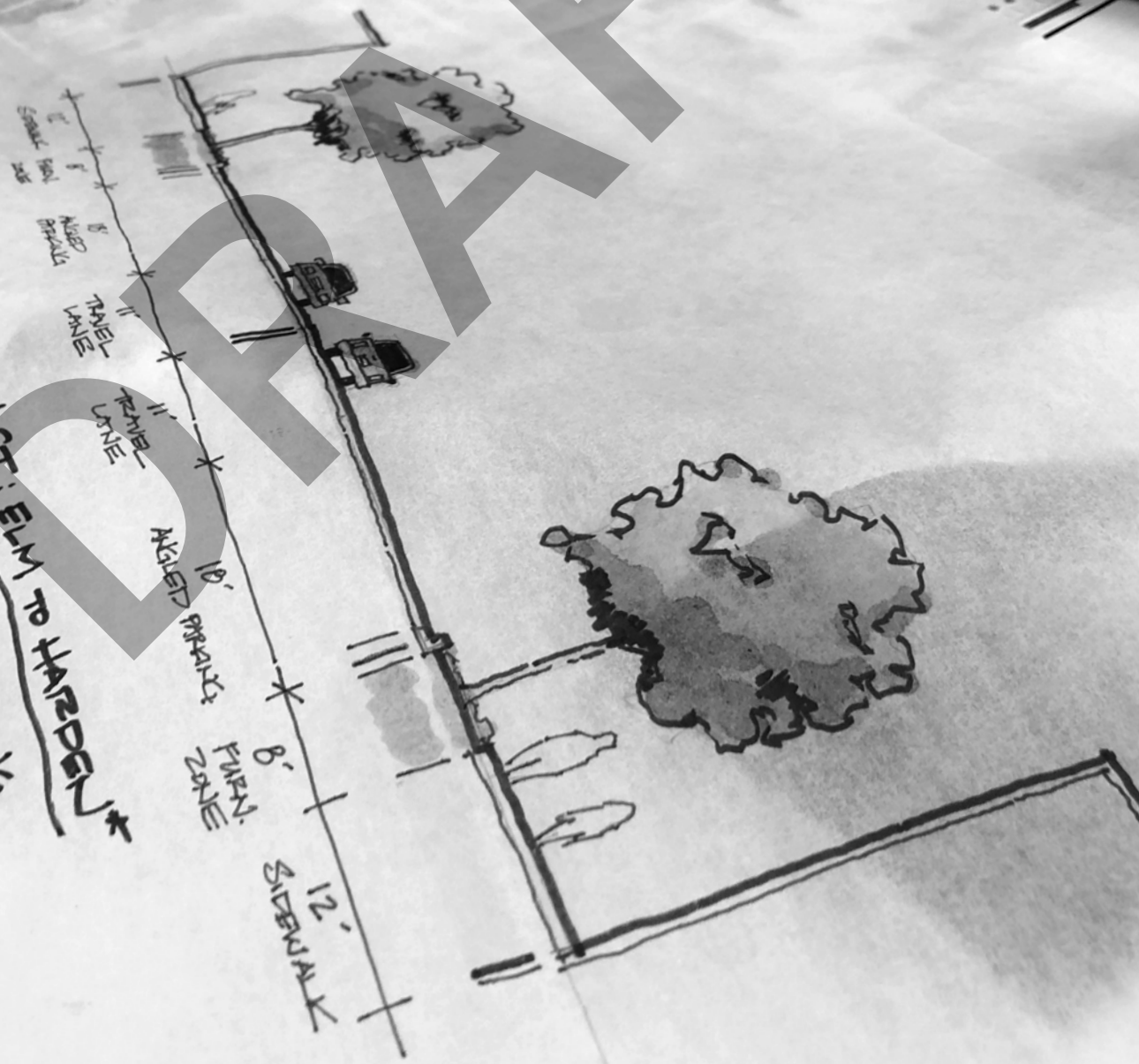
DRAFFES 3

STREETSCAPE + PUBLIC SPACE

- Gateways
- Court Square
- Elm Street
- Main Street
- Green Space/Adaptive Reuse
- Parking



* THIS APPLIES TO S. MAIN + W. ELM
 PROPOSED . . . ANGLED PARKING
 SCALE: 1" = 10'-0"
N. MAIN ST. : ELM TO HARDEN



STREETSCAPE + PUBLIC SPACE

A community's greatest potential for change is within its public realm. Streets make up the largest portion of the public realm. The character and allocation of space on a street plays a key role in user experience. Streets should be attractive, inviting, accessible, safe, and comfortable for all users. Often, this space is underutilized and not serving all users and/or needs in the community. Graham is embracing its potential for creating a vibrant place and taking a great streets approach to revitalization. Great streets place people first – people that drive a car, ride a bicycle, walk, shop, live in, or visit downtown. A people first design approach celebrates life, creates inviting and vibrant public spaces, and provides opportunity for community growth. Investment in the public realm will stimulate private investment. Emphasis on restoring and revitalizing the public realm is woven through success stories in communities across the country. Graham is ready for a catalytic change in downtown that once again establishes the heart of the community.

This chapter provides key insight into the reallocation of space along downtown streets, establishes gateways into the community, and emphasizes opportunities for public gathering spaces. This chapter briefly reviews existing conditions and describes recommendations for each of the following elements:

Project Elements

Gateways:

Gateways serve as transitions and demarcate an entrance to a community. Graham currently has four primary entry points into downtown. Single lane roundabouts will serve as Graham's gateways filled with welcoming landscape, art, and directional wayfinding.

Court Square:

Communities across the country are searching for their postcard photo. For Graham, Court Square is that photo and should be celebrated as the heart of the community. This space must be transformative, iconic, vibrant, and inclusive to all.

Elm Street:

Elm street should serve as a continuation of Court Square and be established as Graham's festival street. Outdoor dining, lush landscape, traditional materials, and catenary lighting will set the stage for weekly farmers markets, concerts, and events.

Main Street:

Main Street can set the stage for Court Square funneling residents and visitors into the heart of downtown through a vibrant tree lined streetscape framing the view of Court Square and providing multimodal connections along this gateway corridor.

Green Space/Adaptive Reuse:

Gathering space is fundamental to life. This is where community is built, children play, and nature is enjoyed. Adaptive reuse of raised buildings turned into fine dining or green spaces establish the tone of Graham's revitalization.

Parking:

On arrival to a destination, parking must be adequate, easily accessible, and connected to the heart of downtown. Parking accessibility, capacity, and wayfinding signage are critical to success.

Please note this plan is conceptual in nature and all recommendations should be done in cooperating with property owners.

Gateways

Four gateways are proposed for the City of Graham Downtown Master Plan. Roundabouts are proposed as the intersection treatment at each of the gateways to reflect the circulation around Court Square and to provide opportunity for public art, branding, or monumentation in the center of each feature. The concept design for the Main Street corridor are discussed in more detail later in this chapter. The following illustrate the concept designs for the four gateway roundabouts approaching Downtown Graham.

Through the planning and design for each gateway roundabout, every effort will be made to minimize the impacts to adjacent property owners.

HARDEN AT EAST ELM

Harden Street and East Elm Street is a skewed intersection with poor sight lines. Currently signalized, the intersection is struggling to accommodate the needs of pedestrians, vehicles, and heavy vehicles that frequent it daily. At one location pedestrians are asked to cross over 130 feet without protection and tractor trailers can not make an eastbound turn onto Harden without the fear of clipping a vehicle in the left turn lane. Due to the configuration of the existing intersection and skew of the roadway, a roundabout is recommended. A roundabout resolves the geometric issues and reduces speeds. While this solution may reduce the risk of crashes and property damage, it also provides a visual gateway upon entering Downtown Graham from the east. The roundabout marks the east entrance into Downtown Graham.



Roundabouts:

Roundabouts are proven to improve safety and mobility for all users, including multimodal, while also maintaining the capacity of the existing intersection. Design includes larger central islands that can be landscaped or hardscaped, splitter islands (similar to pedestrian refuge islands), on the approaches to provide safe crossing locations for pedestrians and bicyclists, and provide a transition of speed through deflection. On average, roundabouts:

- Improve traffic flow
- Reduce overall collisions by 37%
- Reduce injury collisions by 75%
- Reduce fatal collisions by 90%
- Reduce pedestrian collisions by 40%
- Decrease average pedestrian and vehicle conflict points from 16 at a traditional signalized intersection to 8.

(WDOT, IIHS, FHWA, NCHRP 672)

HARDEN AT WEST ELM

The intersection of Harden Street and West Elm Street is a sea of asphalt with numerous curb cuts contributing to driver confusion. Currently signalized, the intersection prioritizes motorists continuing along NC 87 and struggles to provide adequate pedestrian facilities. Due to the configuration of the existing intersection and lack of clarity, a roundabout is recommended. A roundabout resolves the geometric issues, reduces turning speeds, and improves traffic flow. Similar to the roundabout at Harden and East Elm, Harden and West Elm provides a visual gateway marking the west entrance into Downtown Graham.

INTERSTATE BRIDGES



The first visual of the City of Graham that users will encounter are the interstate overpasses. Decorative treatments to the facades will signal to drivers that they are entering the city.

Harden at East Elm



Harden at West Elm



SOUTH MAIN AND MCADEN

A roundabout is recommended at the intersection of Main Street at McAden Street to require residents and visitors to slow down prior to entering the Downtown area and serve as the southern gateway into Downtown Graham. This 120 foot single lane roundabout (60-foot inscribed radius) will be equipped with a truck apron to allow for three-quarter turning movements of the design vehicle (WB-67). The large center island can accommodate a lush landscape and sculptural elements to frame the view of Court Square. A shared use path proposed along both sides of Main Street is proposed to continue through the intersection to promote active transportation for local trips. Splitter islands on all approaches of the intersection allow pedestrians to cross one lane and direction of travel at a time and provide protected pedestrian refuge within the islands.

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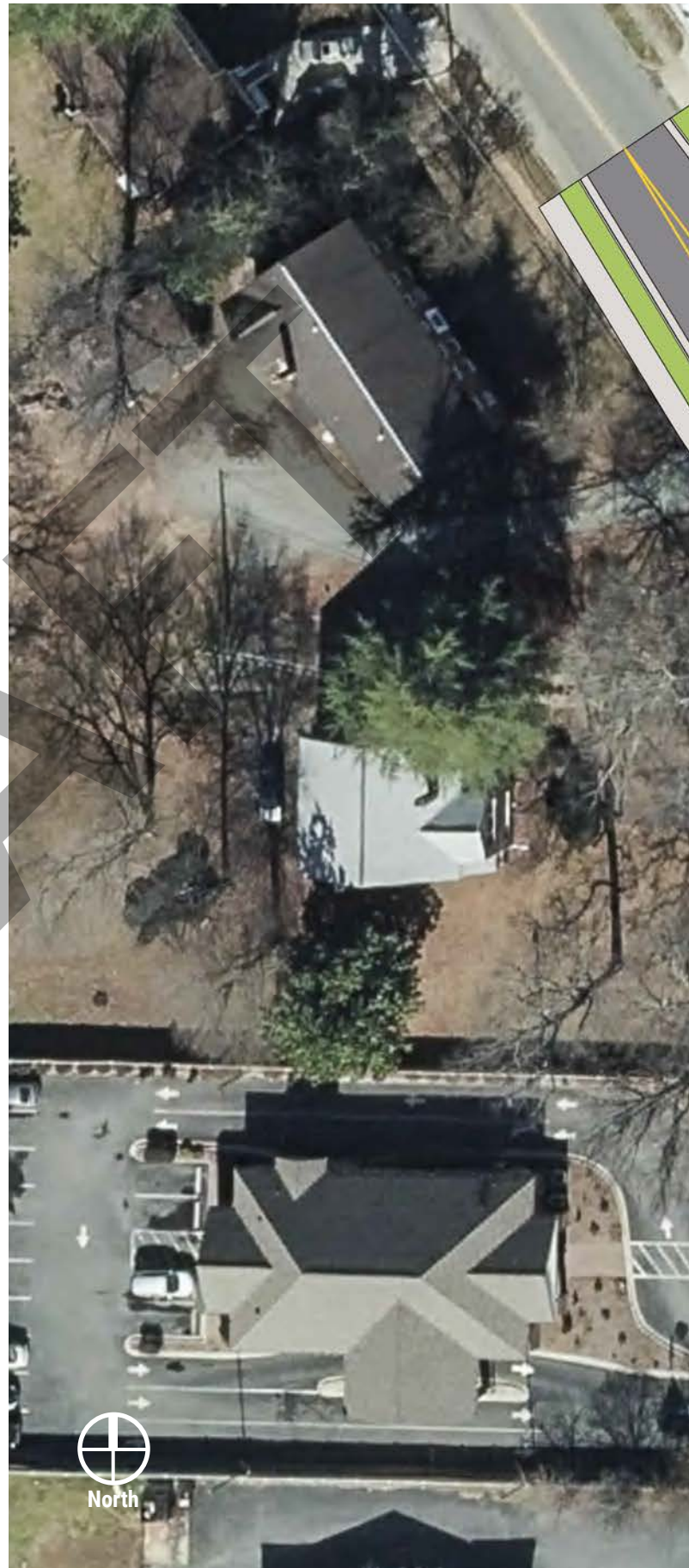


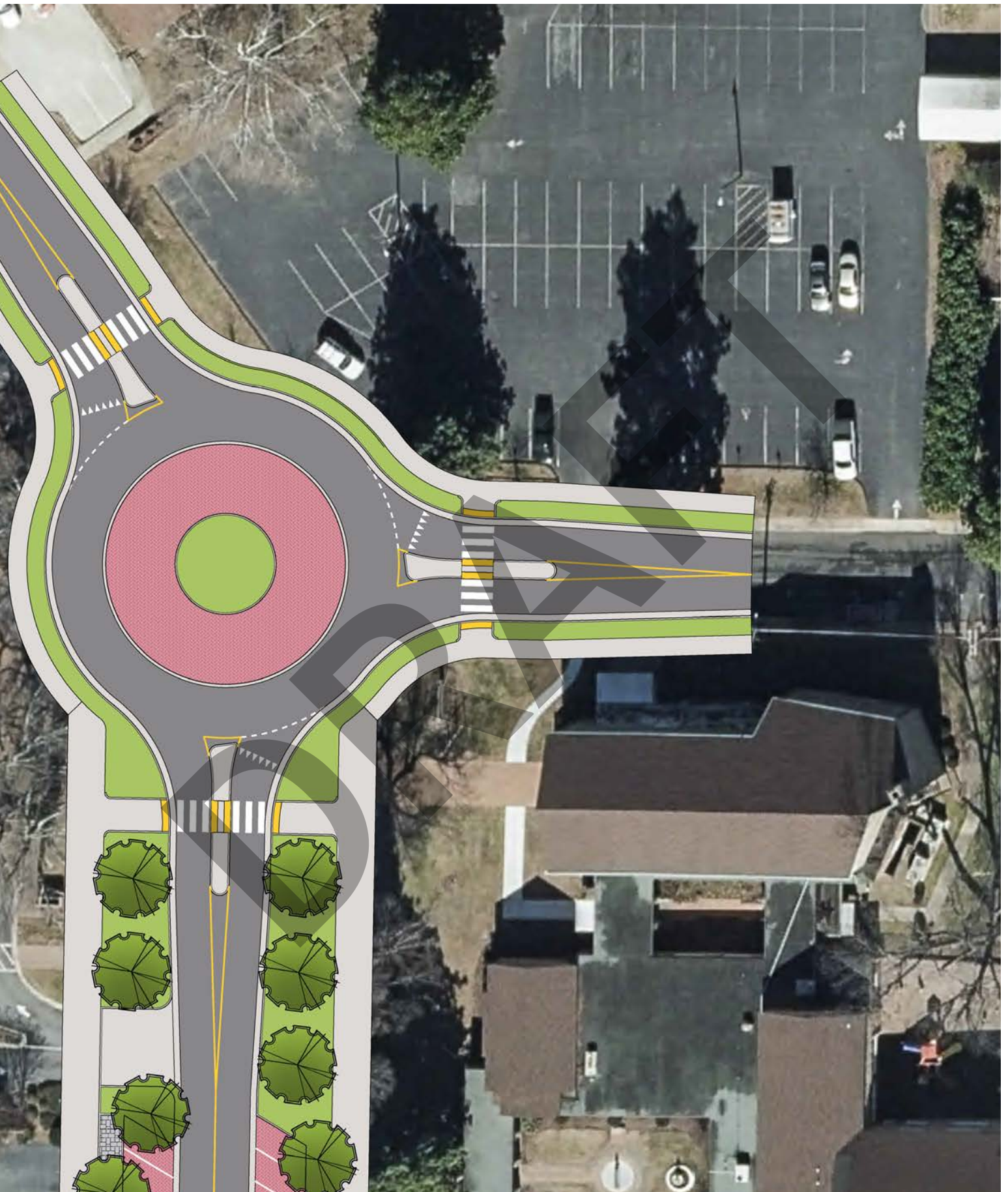


NOT FOR CONSTRUCTION

NORTH MAIN AND ALBRIGHT

Albright Avenue at North Main Street is a skewed intersection that is currently stop controlled for the westbound approach. Main Street is free-flow and introduces an abrupt veer on the street just north of Albright Avenue. Based on city staff review and public comment, vehicular speeds and geometry contribute to an increase in crashes – resulting in a high number of property damage crashes. Main Street transitions from commercial to residential land uses at the intersection of Albright Avenue. Due to the configuration of the existing intersection and skew of the roadway, a roundabout is recommended. A roundabout resolves the geometric issues and reduces speeds. While this solution will reduce the risk of crashes and property damage, it also provides a visual gateway upon entering Downtown Graham from the north. Similar to the roundabout at McAden Street, Albright Avenue marks the north entrance into Downtown Graham.





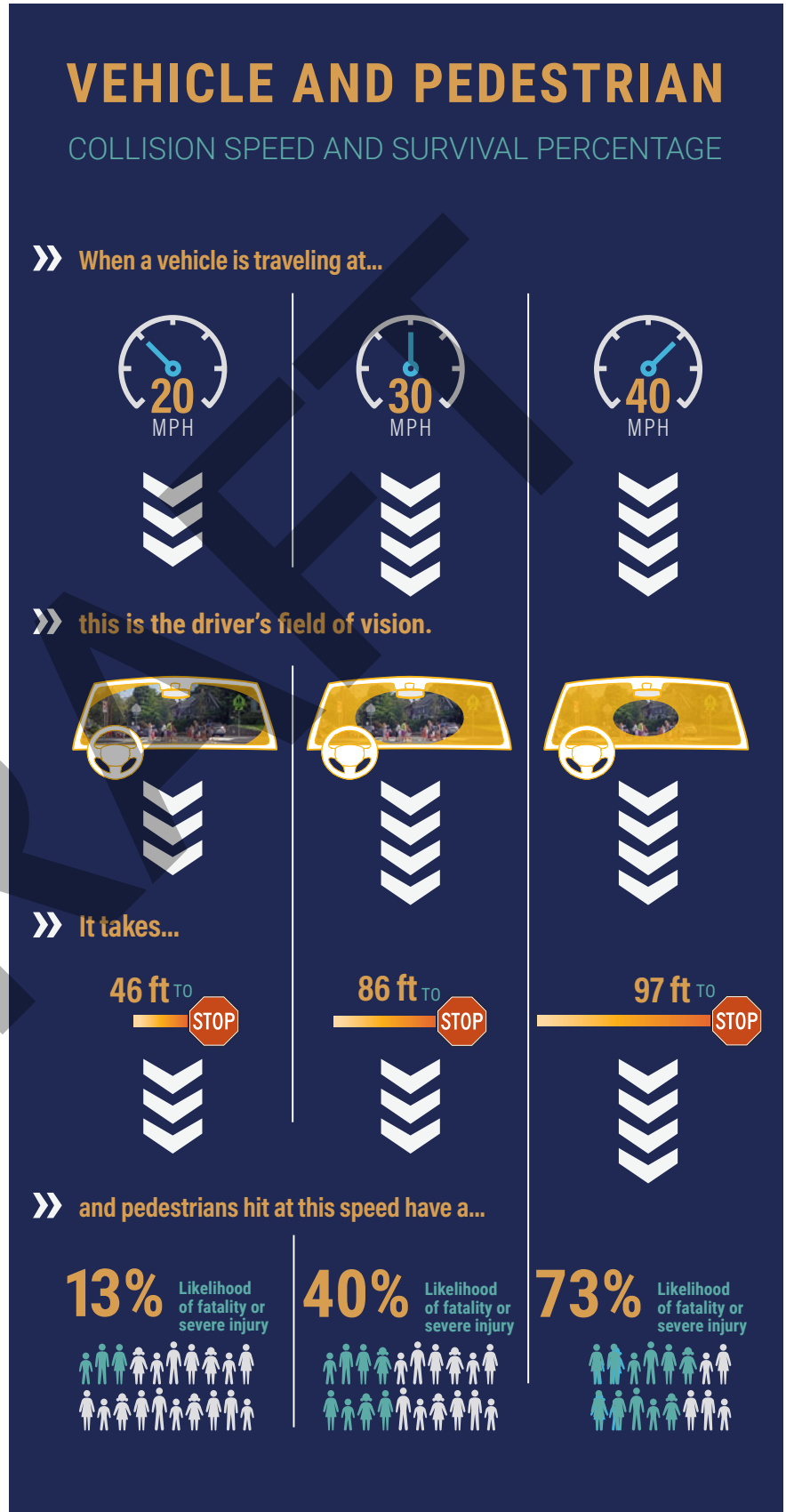
Court Square

Court Square is a paradox of downtown Graham. The Neoclassical quality of the Alamance County Historic Courthouse is a major contributor to Graham’s character and naturally attracts residents and visitors to the Square. However, the current street design and allocation of space makes Court Square an inhospitable place to experience as a pedestrian, bicyclist, or motorist. For Court Square to fully realize its potential as a welcoming destination, the safety, accessibility, and infrastructure issues outlined below should be addressed.

SAFETY

Motorists traveling at excessive speeds makes navigating Court Square dangerous for people of all ages. Public comments at the February 2019 workshop identified pedestrian safety throughout downtown as a theme that needed attention, with multiple comments focused on Court Square. Vehicular speed is a critical factor that affects user comfort and safety. If struck by a vehicle, a pedestrian has a 95% chance of surviving if the vehicle is traveling 20 mph. The chances for survival drop to 10% if the vehicle is traveling at 40 mph.

The lack of street design within Court Square contributes to an unsafe environment for all modes particularly pedestrians. Motorists, including large log trucks, are largely unrestrained by poorly defined and excessively wide travel lanes. Pedestrians hoping to cross Main or Elm Street are facing a nearly 70-foot crosswalk and have little time to judge whether a driver is going to yield, continue through the Square, or turn off onto Main or Elm. The wedge shaped “refuge islands” located at the midpoint of the stamped brick crosswalks offer no physical separation for pedestrians from the adjacent traffic. Bicyclists traveling through Court Square must contend with the high speed, volume of motorist, parked cars on both sides of the Square, and the uncertainty of turning movements onto Main or Elm at high speed.



ACCESSIBILITY



Accessible Crosswalks and Ramps

The lack of up-to-date accessible facilities in Court Square discourage the most vulnerable members of the population from safely accessing and enjoying Downtown Graham. The curb ramps located in Court Square lack required detectable warning surfaces that help visually impaired persons transition between the sidewalk and street. The crosswalks in Court Square are skewed, which require pedestrians to travel longer distances than if the crosswalks were aligned perpendicular to the street. Uncontrolled crossing distances should ideally be no more than 22 feet. Currently the crossings in Court Square are more than three times that distance.

AGING INFRASTRUCTURE



Skewed Intersection

The aging infrastructure in and beneath Court Square is a key driver of this plan and should be addressed to allow for future growth and expansion of Downtown Graham. Fire hydrants located in the Square rely on water lines that are more than 100 years old. Court Square lacks methods for stormwater control, which puts businesses along the Square at risk of flooding during heavy storm events. Shared sewer services run beneath buildings, which is problematic for the existing building foundations and for any future modifications or new buildings.

RECOMMENDATIONS

The redesign of Court Square begins with creating a shared street. A shared street, also known as a woonerf, prioritizes pedestrian and bicycle movements by slowing vehicular speeds and design features that clearly communicate that motorists must yield to all other users (FHWA 2017). Shared streets allow for pedestrians, bicyclists, and motor vehicles to mix within the same space. This is accomplished by a design that encourages low motor vehicle volumes and speeds, is void of elements such as vertical curbs, signs, and pavement markings that separate modes, uses material color and texture changes to define clear zones for pedestrians, and establishes uncertainty of pedestrian and bicyclist movements. This encourages caution by all users, slowing motorist speeds and indicating pedestrian priority. As motorist enter the square, they will be greeted by a ramp to elevate them six-inches to pedestrian level. The proposed design reduces the width of the circulatory travel lane to 15 feet wide and clearly delineates the roadway through the use of brick pavers. Brick pavers provide a necessary visual cue to motorist that they are entering a shared environment. Through the reduction in width and change in street surface material, lower speeds and more predictable vehicular movements will be achieved.

Through reallocation of space additional gathering spaces can be obtained along with opportunities for weekly markets, recreational space, stormwater control measures, outdoor dining, and place making to contribute to the establishment of Downtown Graham as a regional destination. The redesign will provide more direct and efficient pedestrian routes through Court Square. High visibility crosswalks perpendicular to the street will replace the current at-grade, skewed crosswalks. Proper splitter islands will provide pedestrian refuge space, beautification, and aid in the reduction of speed and deflection of motorist entering the Square. Pedestrians will cross one 11-foot travel lane to reach a refuge of the raised splitter island, and the total crossing distance will be approximately 30 feet, rather than the unprotected 70 feet that exists in Court Square today and will still permit access to local trucks and fire engines.

Street trees will enhance the comfort and safety of pedestrians in the Square. Trees provide shelter from the sweltering summer heat, a physical and psychological barrier between pedestrians and motorist, and contribute to the reduction of vehicular speed through ocular narrowing – the narrowing of field of vision. Street trees will help define and reinforce the oval form of the roundabout and will frame views of the historic courthouse from Main and Elm Streets. Planting areas interspersed throughout the Square’s four corners will soften the ground plane and add beauty to the landscape. A lush landscape palette will complement the beauty of the historic courthouse and downtown storefronts.

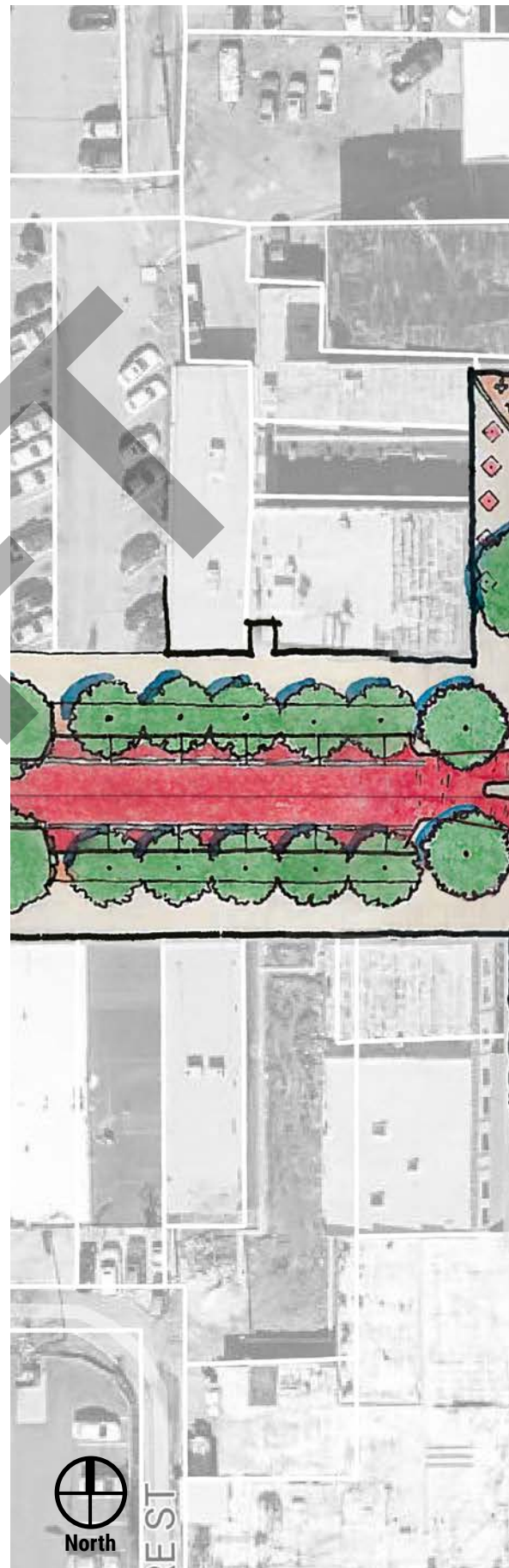
The proposed redesign of Court Square:

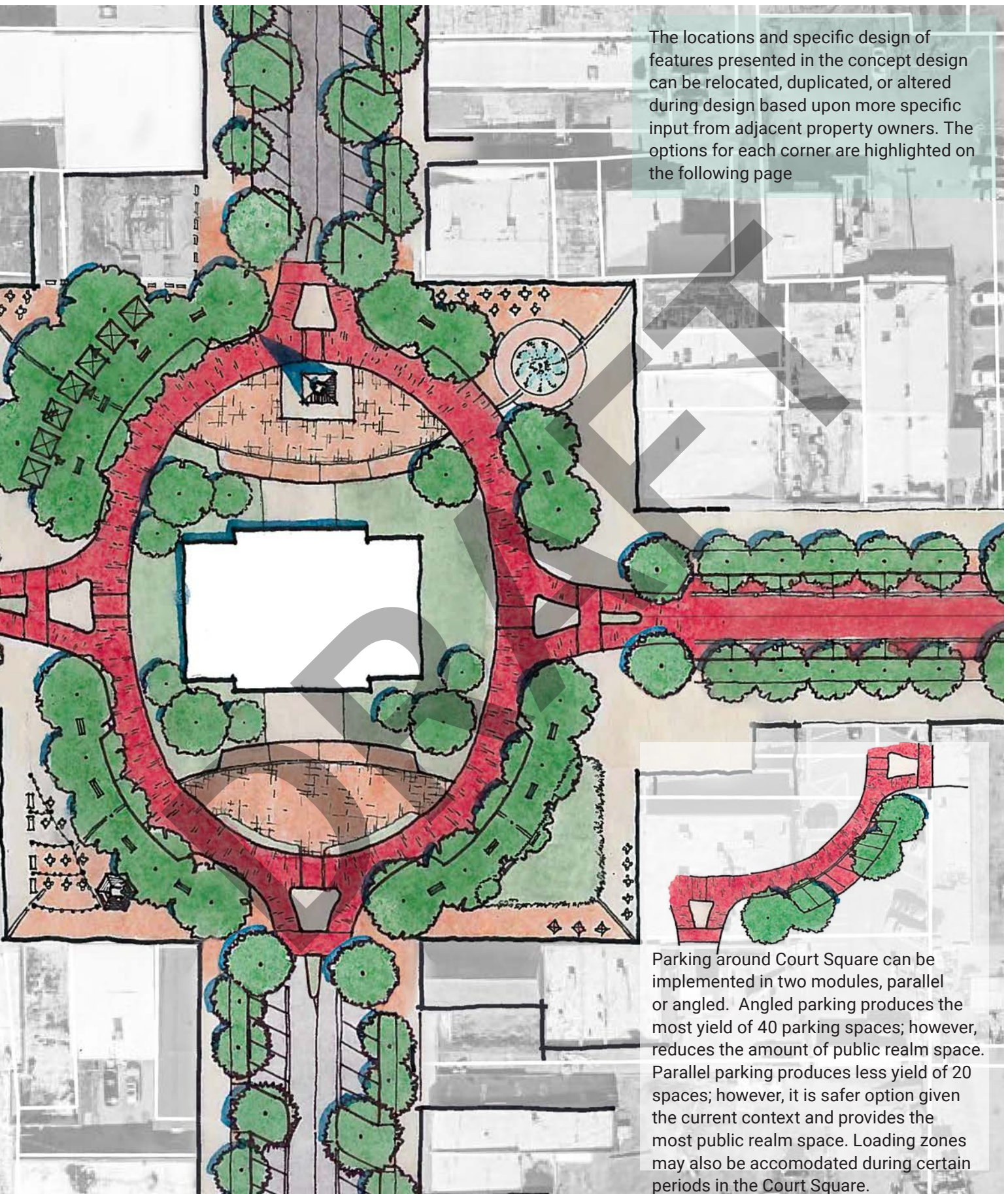
- Enhances the comfort and safety of the built environment for people of all ages and abilities who may be walking, biking, or driving.
- Provides more space for community placemaking where residents and visitors can come together to live, work, and play.
- Better compliments the aesthetic quality of the historic Alamance County Courthouse and downtown storefronts, which defines the small-town character of Downtown Graham.

Overall, the proposed Court Square design will create a more hospitable environment for people who walk, bike, and drive. The roadway design is consistent with FHWA’s best practices for roundabout design, shared streets, and multimodal design. Moreover, the Court Square design is predicated on the assumption that log trucks and other heavy vehicular traffic will be diverted onto alternative routes. NCDOT has signaled their approval of this approach and more discussions will need to be made as the plan moves forward.

Placemaking opportunities include:

- Ample space for cafe seating.
- Opportunities for interactive water features such as splash pads that attract children and provide relief from the summer heat.
- Space for open-air community events that could include farmer’s markets, art, and music festivals.
- Park space that provides opportunities for sitting, resting, and quiet contemplation while enjoying nature.





The locations and specific design of features presented in the concept design can be relocated, duplicated, or altered during design based upon more specific input from adjacent property owners. The options for each corner are highlighted on the following page

Parking around Court Square can be implemented in two modules, parallel or angled. Angled parking produces the most yield of 40 parking spaces; however, reduces the amount of public realm space. Parallel parking produces less yield of 20 spaces; however, it is safer option given the current context and provides the most public realm space. Loading zones may also be accommodated during certain periods in the Court Square.



An open air market is the centerpiece of the northwest corner of Court Square. Ample seating, both bench and cafe, is situated underneath a dense canopy of legacy shade trees.



Water naturally attracts people and is essential to life. A splash pad is recommended in the vicinity of the downtown to have an activity space for children. This water feature will need to be located such that nearby businesses are complementary to the use. At night, the fountain will come to life with integrated LED lights.



European outdoor dining is featured in the southwest corner of Court Square. Catenary lights, legacy shade trees, and outdoor pavillion add contribute to the user experiance within the square.



The option of passive recreation takes the stage in the southeast corner of Court Square with an open lawn. Yoga, dance, kick ball, frisbee, etc. can all take place on the lawn. Surrounding the lawn visitors can find ample seating and shade.

Elm Street

EXISTING CONDITIONS

The current West Elm Street cross section includes approximately a 94-foot right-of-way, with angled on-street parking, 20-foot-wide sidewalks, and a single travel lane in each direction. The wide sidewalks are less generous than they appear at first glance, as the presence of raised tree planters, utility poles, furnishings, and other obstructions reduce the pedestrian clear zone width. Two parking lots on the north side interrupt the urban fabric and introduce conflicts between pedestrians and vehicles.

On the block immediately east of the courthouse, Elm Street appears disjointed because the north and south sides of the street have noticeably different characteristics. On the north side of the street, two curb cuts introduce pedestrian-vehicular conflicts, and the presence of parking lots interrupts the urban fabric established elsewhere in Square and on Main Street. Angled parking is available on the north side, which fits the dominant pattern seen throughout the rest of Downtown Graham. On the south side of East Elm Street, several elements detract from the block’s character, including a narrow sidewalk, several jogs in the sidewalk, wide curb cuts, and parking lots. This block features both angled and parallel parking. An unconventional loading zone for delivery trucks is located in the middle of the roadway near the Square, and a left turn lane is provided for motorists who want to head north on Marshall Street.

Sidewalk Zones:

Sidewalks are not simply the space between the travel lane and storefronts. They are a critical component of the street and public realm comprised of three distinct usage zones each performing specific tasks.

- Frontage Zone**
 The frontage zone is the area that immediately abuts a storefront. This zone may include outdoor retail displays, café seating, awnings, and sandwich signs.
- Pedestrian Zone**
 Also known as the “walking zone”, this zone is the portion of the sidewalk that is used for active travel. It must be clear of obstacles, accommodate anticipated volume of users and users with disabilities, and be a smooth clean surface.
- Furnishing Zone**
 Also known as the “landscape zone”, is the area between the curb and the pedestrian zone. This zone may include landscaping, street furniture, utilities, and street lights.



RECOMMENDATIONS

The proposed design of Elm Street reimagines a street focused on ensuring safety for all modes of transportation, providing pedestrian amenities, and creating an attractive corridor that invites new development and highlights existing businesses. These key elements will be achieved through a cross section that allocates more space to pedestrians and provides opportunities for Elm Street to serve as Graham’s “festival street.”

Curb extensions will bookend Elm Street at its intersections with Maple Street, Marshall Street, and Court Square. Curb extensions for tree islands and stormwater management are also proposed at midblock locations. The curb extensions will narrow the street width and promote lower vehicular speeds as motorists enter the heart of downtown. Brick pavers will be used as the roadway surface material, which will help identify the 100 blocks of Elm Street and Court Square as the heart of Downtown Graham. In addition to contributing positively to Graham’s aesthetic character, the visual contrast and textural differences of brick pavers will signal to motorists that lower speeds are intended.

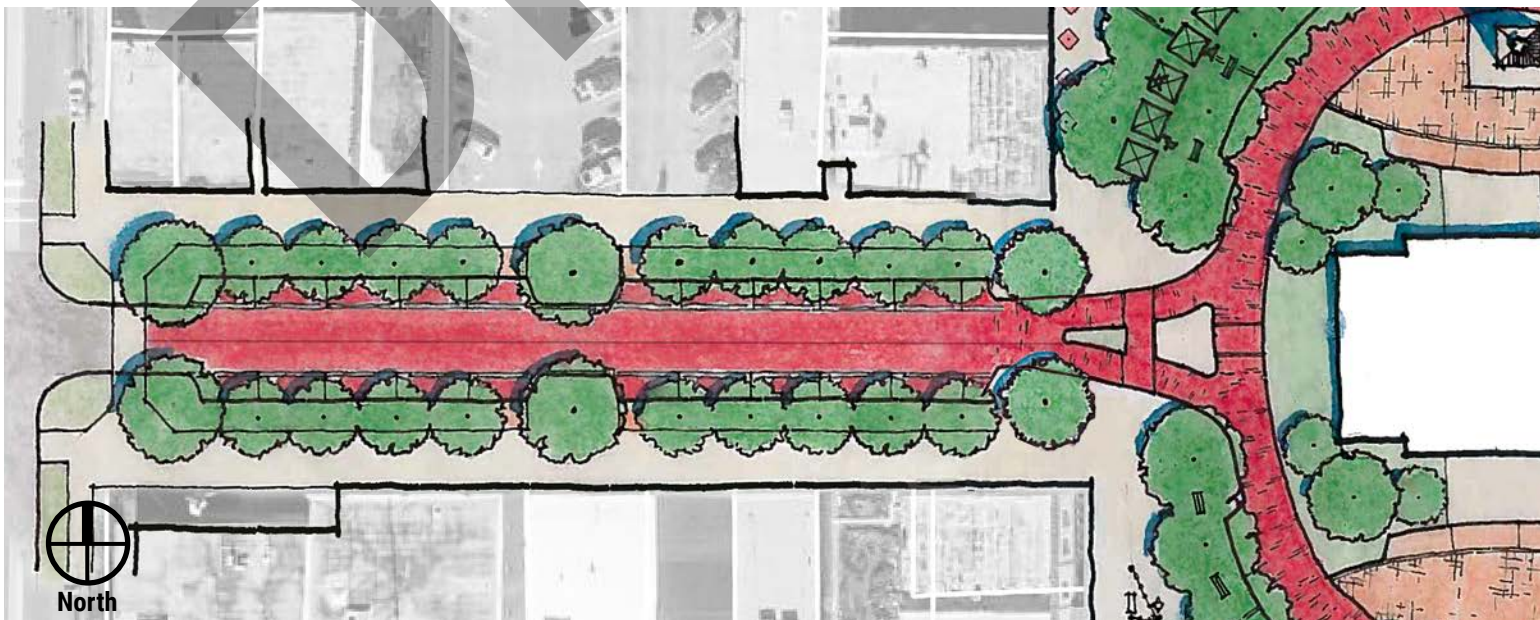
Sidewalks will be concrete to provide a comfortable and safe walking surface, and the overall sidewalk width will be widened to 28 feet. Street tree plantings will consist of larger, legacy trees and medium-sized ornamental trees located in the furnishing zone. Legacy trees are envisioned to be trees that speak to Graham’s natural or

cultural heritage. They will be located in the expanded soil volume provided by curb extensions. Ornamental trees will be located between each curb extension and will contribute an attractive seasonal quality, such as spring blooms or fall color.

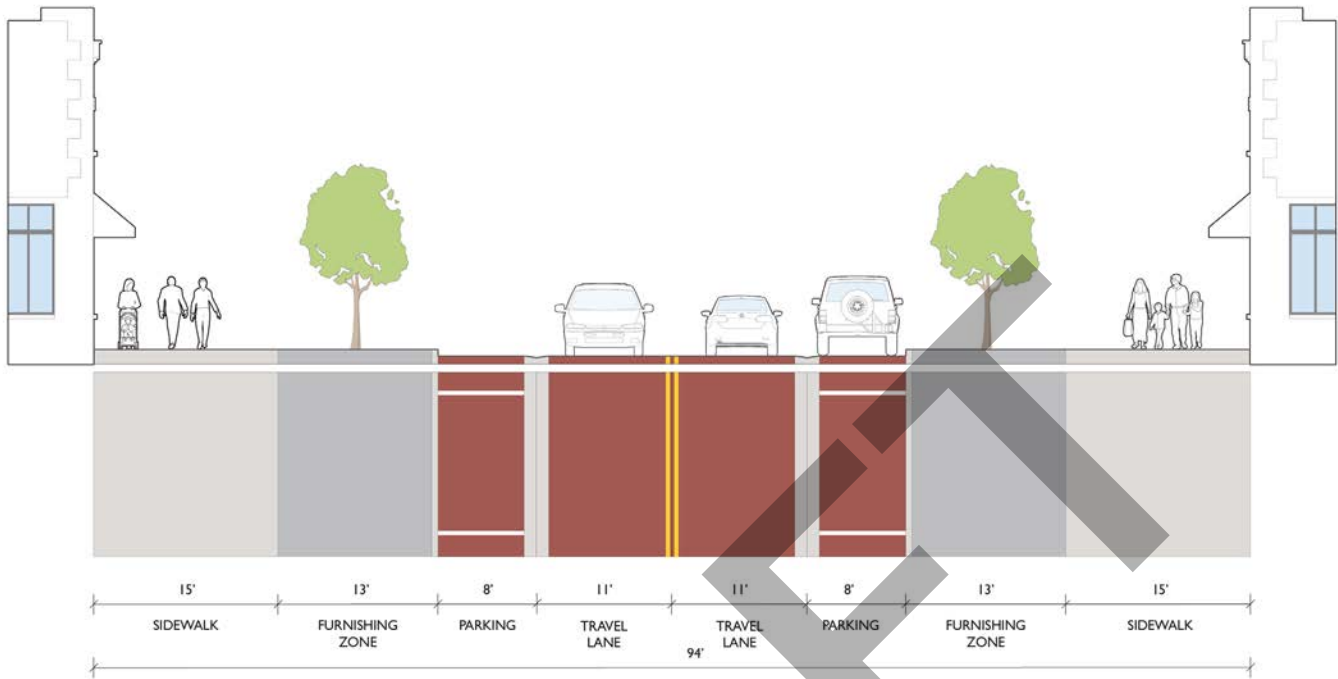
Elm Street is envisioned as Downtown Graham’s festival street. The City currently owns the street and can close the street to vehicular traffic during festivals and events without additional agency approval. Through travelers during festivals have two alternative east west routes, Harden Street, which is one block north of Elm, serves as the primary arterial and pass-through for motorists traveling east-west through Graham. If the City desired to keep vehicular traffic open during events, the generous width of the pedestrian realm would allow Elm Street to function as a festival street.

Parallel parking should replace the angled parking that currently dominates the 100 block of Elm Street. The additional real estate gained will allocate more space to pedestrians and the resulting sidewalk width will allow Elm Street to function as a festival space regardless of whether the street is closed to vehicular traffic during events. A more comprehensive discussion of the proposed parking strategy for downtown Graham is located within this chapter.

Proposed Elm Street Concept



Proposed Elm Street Cross Section



- An 4-foot-wide frontage zone, which will allow businesses to provide café seating along the face of their buildings,
- An 11-foot-wide pedestrian clear zone, which is an appropriate width for a downtown setting,
- A 13-foot-wide furnishing zone, which will accommodate street trees, utilities such as street lights, sign posts, and utility poles, and temporary sidewalk signs



WEST ELM - BEFORE



WEST ELM - AFTER



Main Street

Main Street (NC 87) is a primary route into Downtown Graham. It is the direct connection between Interstate 40/85 and Court Square and is home to the commercial center of Graham. Traffic volumes vary along Main Street as the street transitions from the interstate into the heart of the community. The corridor is home to a variety of land uses, including commercial, mixed-use, and residential. It is important that recommendations mitigate impacts to property owners along the corridor while planning for future growth and redevelopment in downtown. Main Street is currently owned and maintained by NCDOT, a valuable stakeholder throughout the planning process for the City of Graham Downtown Master Plan. Additional analysis and coordination will be required prior to implementation of the recommendations that follow.

This chapter provides brief descriptions of the recommendations along Main Street to aid in the creation of gateway corridors into Court Square. Through reallocation of space, prioritization of all users, and reimagining of the existing street, this chapter further explains how the concepts can transform Downtown Graham.

Design Elements

Gateways:

Gateways highlight key features that already exist in the City. By providing an aesthetically pleasing and consistent treatment upon entry to downtown, users will be pleasantly reminded of where they are headed, what to expect ahead of them, and why they chose this route through Graham.

Main Street Corridor:

Main Street currently services through traffic in Graham. Through a reimagined streetscape people will be drawn to the heart of downtown. Tree lined streetscapes, multimodal considerations, and prioritizing the local user provide the perfect frame for the historic Court Square.

Approaching the Court Square:

North and South Main Streets converge at the Court Square. The final block for these streets are critical in setting the tone of for a vibrant downtown. Great streets are designed to be inviting for all users – the driver on an afternoon cruise, the family bicycling for an ice cream cone, and the resident on a walk after a long day at work.

GATEWAYS

Two of the four gateways for Downtown Graham are along the Main Street corridor. While each gateway is described in more detail in a previous section of this chapter, it is important to emphasize that these features along Main Street visually and physically mark transitions into the core of the community. The proposed roundabouts provide for more than just traffic calming but offer space for public art or monumentation that has the potential to be viewed from the interstate for the South Main Street and McAden Street gateway.

MAIN STREET CORRIDOR

In tandem with the recommendations to Court Square and the gateway intersection improvements, it is important to provide a vibrant streetscape for Main Street from the gateways at McAden Street and Albright Avenue. Drivers and users should be informed that they are entering a shared, people-focused space and that speeds should reduce. One way to accomplish this is through visual and physical cues as part of the streetscape design. The additional width gained through reallocating space on the street can be used for shared use paths, additional parking, staging areas for visitors, and gathering spaces. The following section will take a closer look at the recommended changes along Main Street as well as Pine Street.

Existing Conditions

As it stands today, Main Street has multiple travel lanes and often wide outside lanes. Public and city comments regarding Main Street were consistent throughout the engagement process – vehicle speeds are in excess to speed limits, logging trucks make movements unsafe, and the current conditions encourage through traffic. Through the reallocation of existing lanes and asphalt width, the view to the courthouse can be further emphasized, vehicular traffic can be slowed, and the focus can be on the local user.

Proposed Main Street Corridor

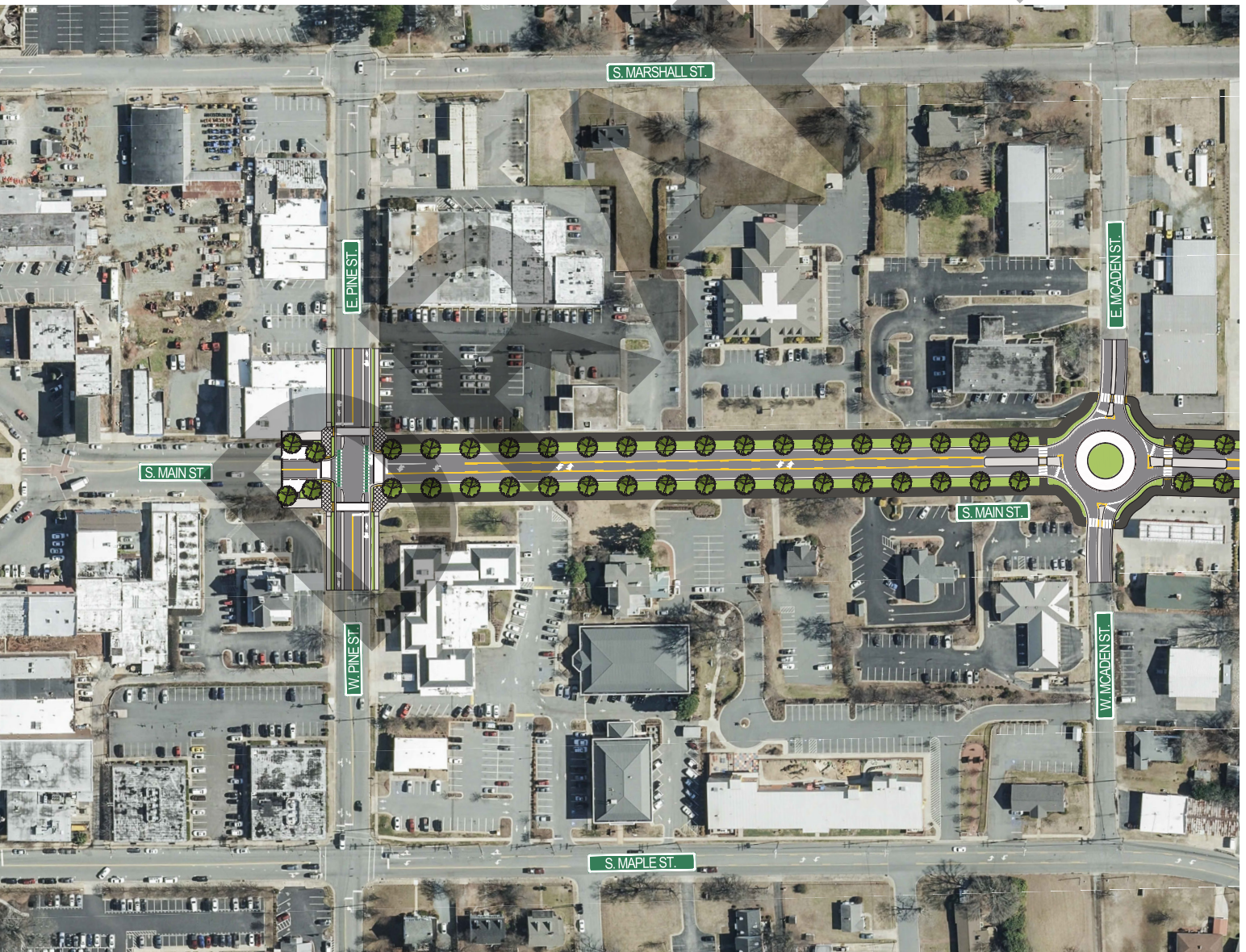


Recommendations

Through strategic narrowing and lane reductions, addressing the public's comments can be achieved. The reclaimed width can be planted with large canopy trees providing visual cues to slow vehicle speeds while providing an attractive streetscape. A shared use path is recommended along both sides of Main Street, from Interstate 40/85 to Pine Street, to provide multimodal options while redefining Main Street's focus on all users. From Gilbreath Avenue to McAden, Main Street will taper from a five-lane section to a three-lane section. North of the proposed roundabout at McAden, the three-lane section is maintained and eventually drops the left turn lane at Pine Street. Maintaining fewer travel lanes reduces the distance for pedestrian crossings at intersections and allow ample space for landscaping. The corridor would also have the opportunity for angled parking if designed.

A two-lane section is then maintained through Court Square eventually transitioning at the Albright Avenue roundabout. Parking along North Main will be consistent throughout and continue to provide access to nearby businesses and properties.

It is important to note that through the reallocation of travel lanes and existing width, the land-use context north and south of Court Square will also begin to change. Through the provision of multimodal connections, safe walking routes, and reduced traffic speeds, the existing footprint becomes more inviting for future investment and growth outside of the historic 100 blocks. The City should consider updating landuse regulations along the approaches to the Court Square to reflect the downtown character in conjunction with the design and implementation of the gateway corridors.

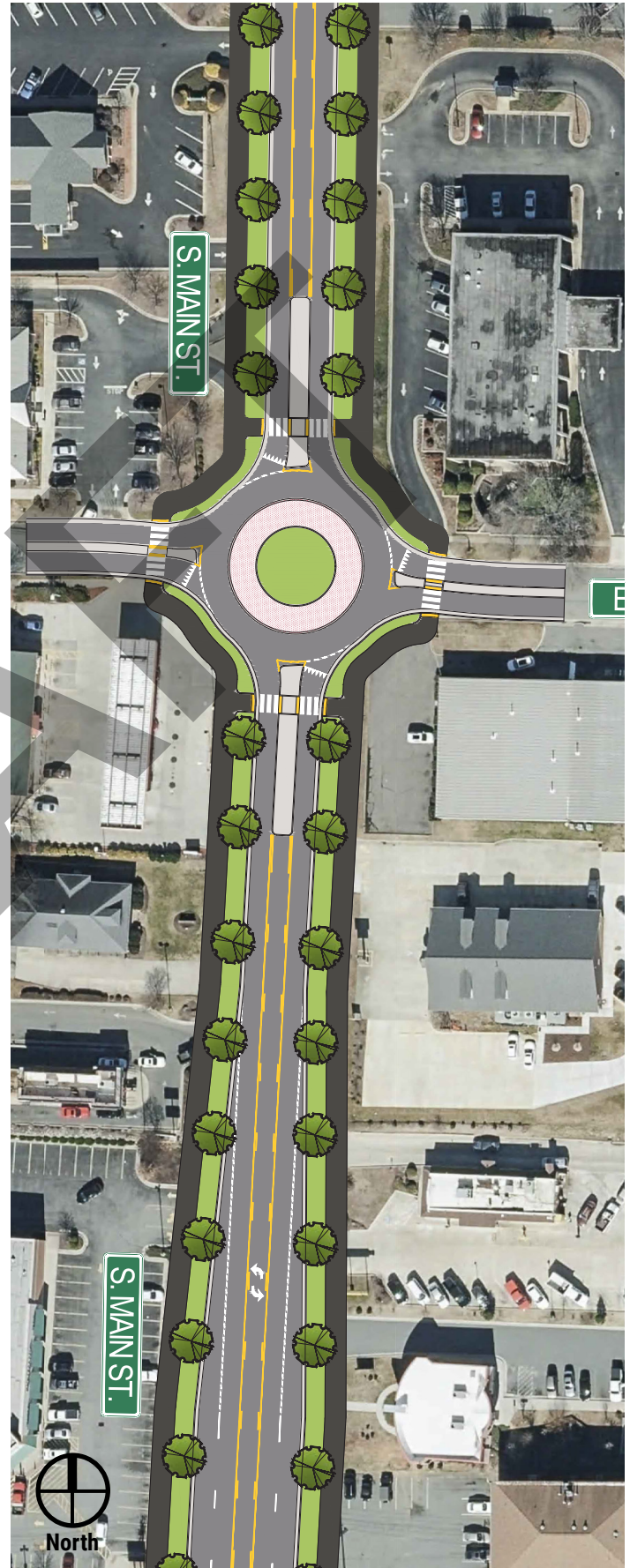


Gilbreath Street to McAden Street

Residents or visitors approaching Downtown Graham from the south may first realize how close they are to the Court Square as they approach the Gilbreath Street and S. Main Street intersection. Encircled by retail and commercial development, this intersection collectively sees approximately 30,000 vehicles per day from all approaches. Gilbreath serves as the first east west connector to Graham's grid network and provides access to residential development on both sides of Main Street. This intersection also gives a first glimpse of the historic courthouse and should serve as a transition from the interstate context along the street toward a more urban and small-town context.

The intersection of Gilbreath should be designed to allow for several vehicular movements and create a decision point for automobile drivers on whether they desire to go to or around Downtown Graham. A truck bypass is proposed to begin at this intersection directing tractor trailers around Downtown Graham. North of the Gilbreath Street intersection, it is recommended that S. Main Street narrows to a three-lane section with shared use paths on both sides as it approaches the gateway roundabout intersection at McAden Street. North of McAden the three-lane cross section continues with wide landscape areas for street trees that frame the view of the courthouse. The "Before" and "After" images to the right illustrate how the Main Street and McAden intersection is visualized with the proposed concept design.

Proposed South Main Street



SOUTH MAIN STREET GATEWAY - BEFORE



SOUTH MAIN STREET GATEWAY - AFTER



McAden Street to Pine Street

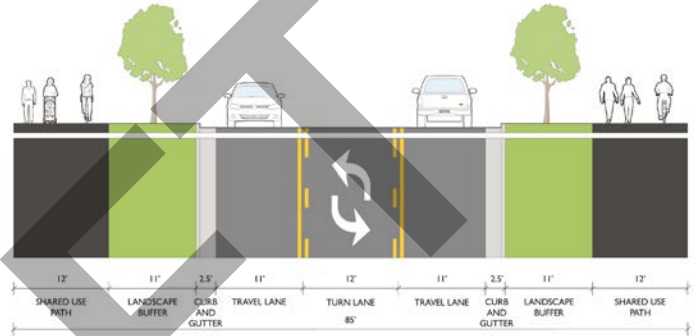
Pine Street is an east-west connector south of the 100 block of downtown, acting as an alternative route across downtown and a local connector to neighborhoods and Graham Middle School. The roadway cross section varies between two- and three-lanes of travel expanding in pavement width between Maple Street and Marshall Street as it approaches Main Street.

A bicycle striping project is currently planned along Pine Street. To increase safety for all bicycle users, it is recommended that between Maple Street and Marshall Street the bicycle facility is upgraded to a separated bike lane. Separated bike lanes provide both a vertical and horizontal component between the bike lane and the adjacent vehicular travel lane. Working within the existing curbed limits, the provision of a 4-foot raised buffer and a 6-foot bicycle lane can be accommodated. Updates to the intersection at Main Street include removing left turn lanes along Pine Street, continuing shared-use paths along Main Street, high visibility crosswalks, and conflict

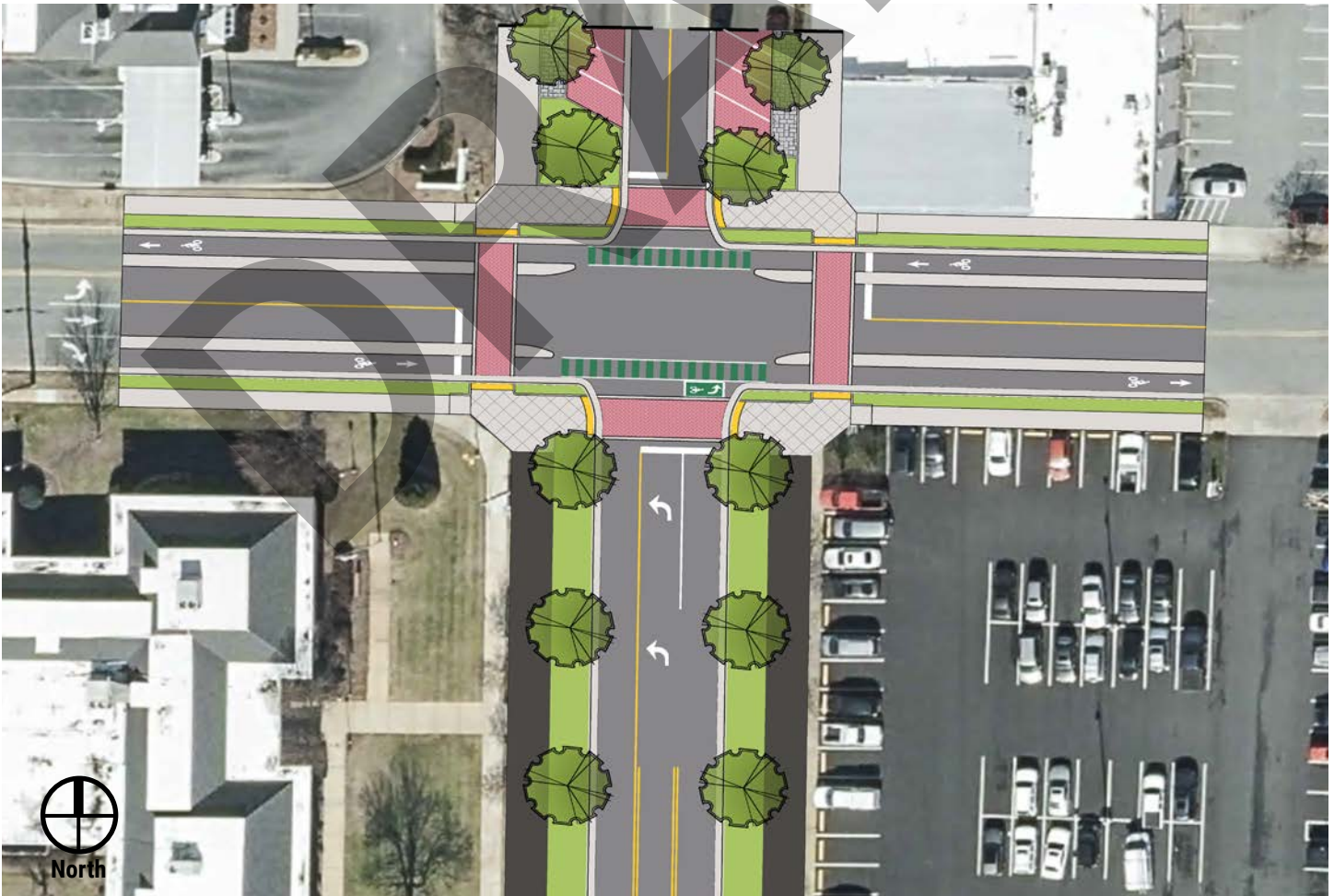
markings for bicycle connections. Conflict marking are also recommended at all driveways along Pine Street where the raised buffer is disconnected for vehicle access. It is recommended that Pine Street remain a signalized intersection.

The “Before” and “After” images to the right illustrate how the South Main Street and Pine Street intersection is visualized with the proposed concept design.

Proposed S. Main Street Cross Section



Proposed Main Street Intersection



MAIN STREET/PINE STREET - BEFORE



Safety should be priority when designing the streetscape. Additional considerations for storefront access/visibility may be important when laying out streetscape elements (i.e., trees, lighting, and furnishings).

MAIN STREET/PINE STREET - AFTER



Albright Avenue to Harden

The gateway roundabout at Albright Avenue and N. Main Street presents an opportunity to establish an attractive streetscape that invites users into downtown from the northern part of the community. Proposed changes increase the number of street trees along North Main Street as it approaches Harden Street and updates on-street parking striping to ensure that each stall is adequate width and depth. On-street parking is proposed to be angled parking and the two travel lanes are maintained between Albright Avenue and Harden Street. Curb extensions are proposed at the Harden Street intersection to shorten pedestrian crossing distances and lower turning speeds for vehicles.

Proposed N. Main Street Concept Section



DRAFT

APPROACHING COURT SQUARE

The limits of this section are along Main Street, from Pine Street to Harden Street. These blocks mark another transition into Court Square from both North and South Main Street.

Existing Conditions

The current Main Street right-of-way from Pine Street to Harden Street is consistently 98 feet, with angled on-street parking north of Court Square and parallel parking to the south, variable width sidewalks, and a center turn lane. In the section between Court Square and Harden Street, the center lane acts as a loading zone for local businesses. Sidewalk width is often impeded by existing light poles, refuse bins, and other furnishings. Travel lanes consistently exceed standard widths and are a byproduct of the heavy truck traffic, especially logging trucks, that passes through downtown. Pedestrian crossings are limited to the intersections at Pine Street, Harden Street, and the traffic circle at Court Square. Due to the existing pavement width of approximately 68 feet, pedestrian crossing distances are long and often unsafe. Mid-block crossings movements are also occurring, using the turn lane/loading zone as a place of refuge.

Recommendations

The proposed design of Main Street for the approaches to the Court Square provides a street that is grounded in increasing the safety of all modes of transportation, re-igniting the vibrancy of the historic square, and maintaining routes as they are today. No through travel lanes in approach to Court Square are recommended for removal. The existing three-lane section will be reduced to a two-lane section, removing the center turn lane and loading zone, and the reclaimed width will be utilized

to further promote public gather space, pedestrian walkways, and new streetscape approaching the square. Loading zone areas can be accommodated through regulation of on-street parking areas to ensure businesses are adequately served.

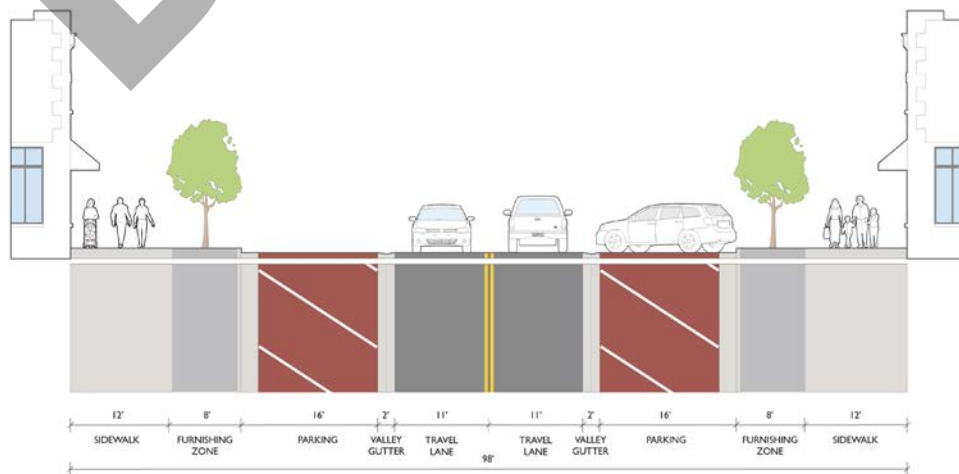
The intersections at Pine Street and Harden Street will be reduced in size. This reduction will translate to shorter pedestrian crossing distances, reduced vehicular speeds, and will elevate awareness of motorists of the presence of pedestrians. Proposed crosswalks should be inlaid with brick to further emphasize slow speeds in the downtown area. Unlike Elm Street, the two travel lanes of Main Street will remain asphalt. Identification of entering the 100 blocks of Main Street will be achieved through human-scale furnishings, improved crosswalks, and an overall reduction in pavement width.

Sidewalks will remain concrete. Sidewalks are recommended to maintain a width of 12 feet abutting store fronts on each side of Main Street but the effective width will feel much larger due to additional streetscape elements that include:

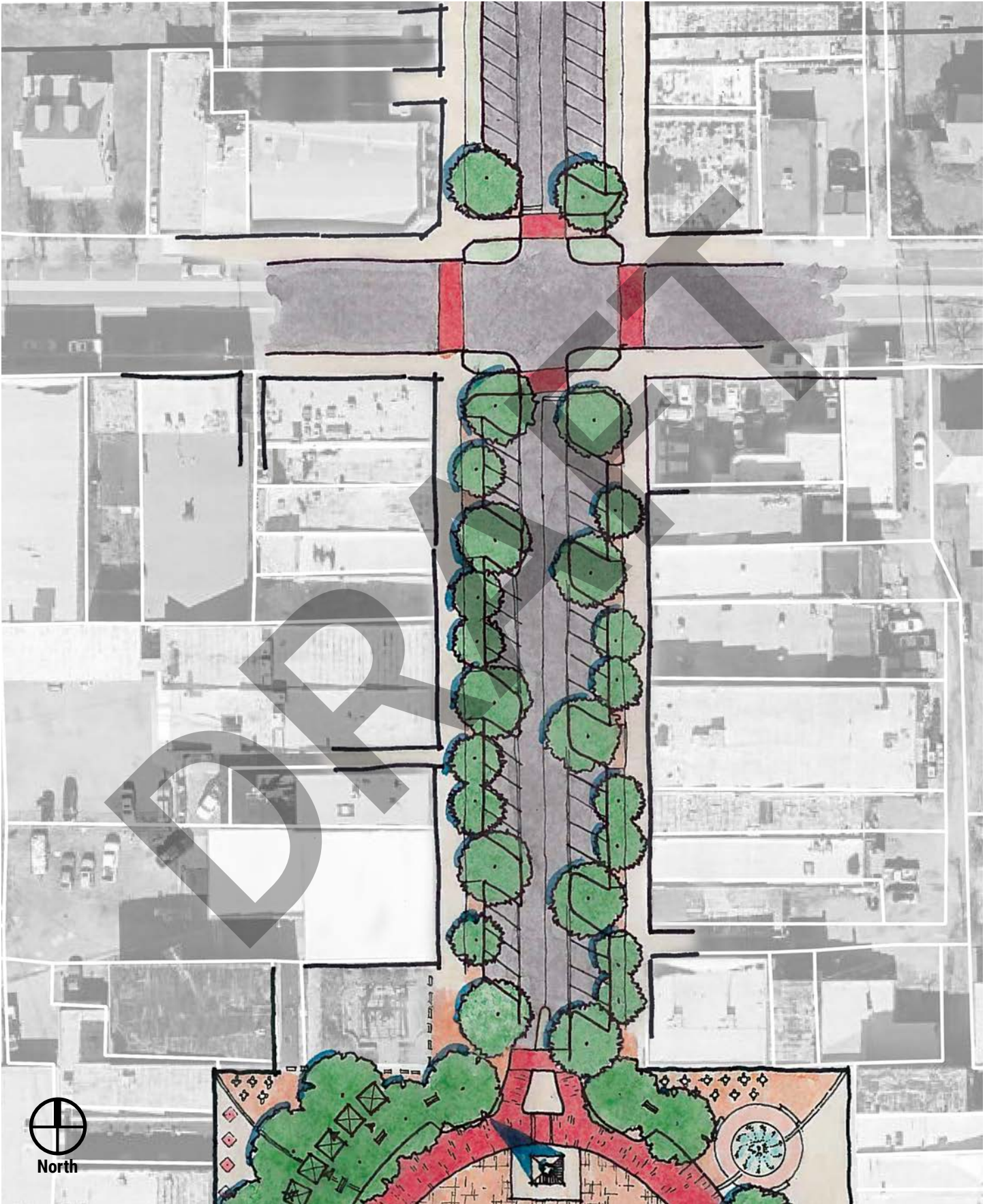
- An 8-foot-wide furnishing zone, accommodating street trees and furnishings, and through use of pavers will visually separate the pedestrian realm from parking, and
- An 18-foot-depth angled parking adjacent to the travel lanes.

During the public engagement process, angled parking was the selected parking recommendation along Main Street. Parking for the entire downtown area is discussed in more detail in the following section.

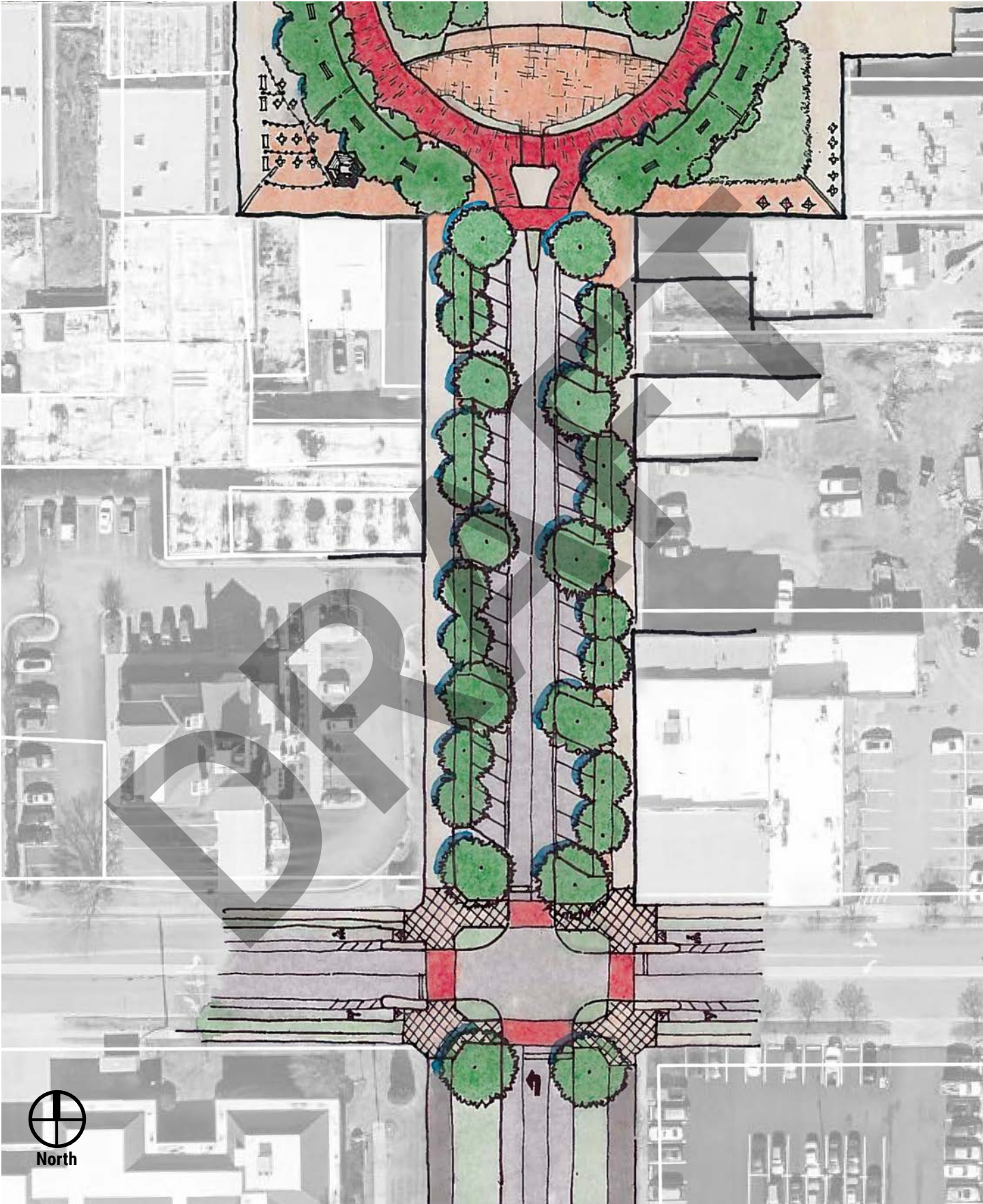
Proposed Main Street Cross Section (Albright to Pine)



NORTH MAIN STREET CONCEPT (100 BLOCK)



SOUTH MAIN STREET CONCEPT (100 BLOCK)



Green Space/Adaptive Reuse

Throughout the public engagement process, community members and city leaders cited a need for more green space in downtown. Citizens and visitors desire active engagement outdoors in green spaces, long to experience nature, and need a reprieve from the built environment. Graham's downtown is full of possibilities outside of Court Square to provide these gathering spaces through adaptive reuse of vacant parcels or surface parking lots. Green Space will help support local economies, attract business investments and tourism, reduce crime, and improve pedestrian safety and public health. Public gathering spaces can provide a sense of character and identity to downtown Graham.

Community members and city leaders also identified a need for a boutique hotel and event center within downtown. Currently, there are limited hotel accommodations within the City of Graham. Visitors

are forced to find accommodations in the neighboring communities of Burlington and Mebane. Graham is missing out on this economic opportunity to capture visitors looking for weekend destination full of entertainment and dining experience in a welcoming downtown environment. A boutique hotel will attract a higher clientele to Graham, compliment the existing architecture, and not detract from the character that is Graham. Additionally, an event venue capable of accommodating up to 500 attendees should be located within a 5-minute walk to Court Square.

During the design workshop and public engagement processes, the design team analyzed parcels of land to determine possible locations for green space, a boutique hotel, and event venue. The following criteria were used to assess how suitable various parcels were for being transformed:

Suitability Criteria

01

Proximity to downtown: To be a cohesive part of the community's downtown fabric, parcels should be close to downtown (i.e., within a quarter mile of the Court Square).

02

Access to pedestrian or planned bicycle pathways: Facilities should be easily accessed from downtown by connected sidewalks and bicycle infrastructure.

03

Vacant vs occupied parcel: It is often easier and more financially feasible to transform vacant, underdeveloped, or undeveloped parcels into greenspace or buildings than it is to do so with existing structures and denser development. While parcels with existing structures were considered, they would ultimately be more challenging to implement and must maintain their historical character.

04

Parcel size: The parcel must meet the needs of its desired use and fit the desired context of Graham.

05

Parcel orientation: Some parcels may not be suitable due to their orientation; if the parcel is not facing the street, is irregularly shaped, or contains challenging natural features, it may not be feasible for development.

06

Parcel assembly: In certain cases, a desired parcel size may be achieved through the assembly of small parts of several existing parcels. This can result in a more customized size and orientation, but the process of assembling parcels can also be long and difficult.

07

Opportunity cost: As with any public use, there is an opportunity cost for developing a parcel in a place where another use could be. It is helpful to consider the existing zoning and communicate with community members and city staff to determine what costs are important to consider during the planning phase.



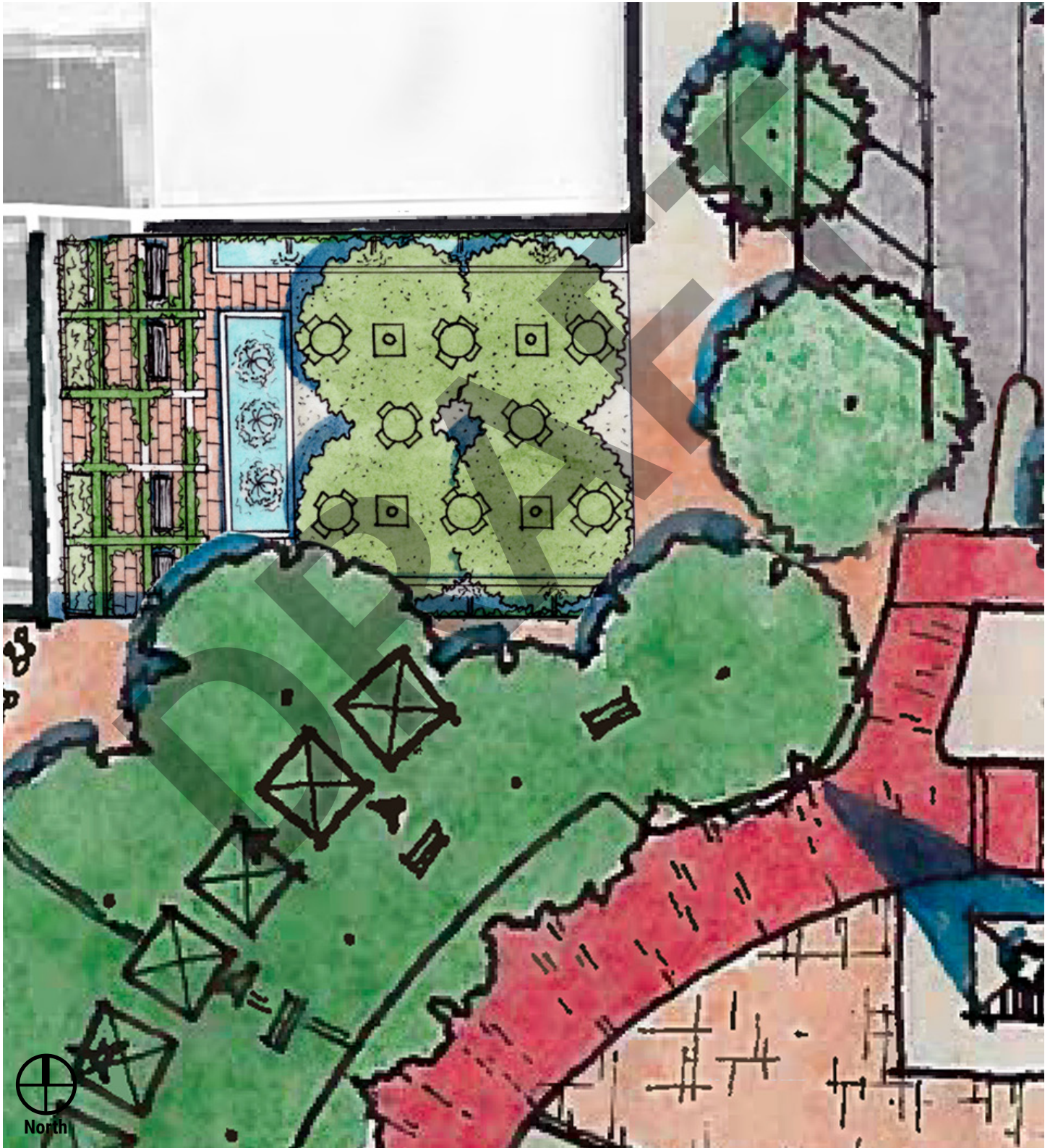
Green Space and Adaptive Reuse Map

The Green Space and Adaptive Reuse presented on this page are suggestions of alternative use. City should work with property owners or look at incentives for redevelopment.

- 1 Underdeveloped lot and tractor parking lot: On the southeast 100 block, there is a partially vacant parcel between Mary's Hair Salon and Covenant of Love Christian Church. This parcel has potential to be integrated with the aforementioned boutique hotel. Its Main Street-facing orientation provides great opportunity for further activating the pedestrian realm in downtown, and there would be an opportunity to connect this parcel to destinations to the east by consolidating parts of other parcels as needed.
- 2 Repurposed parking lot on West Elm Street: Between the Roasted Coffee Depot and an attorney's office are two parcels that are street-facing parking lot entrances today. Parking lots placed along a festival street is not the highest and best use of such valuable downtown property. A public-private partnership should be considered to develop the property to add to the downtown appeal. These parcels are attractive due their proximity to Court Square and because there are presently no structures on these lots. Removing these entrances to parking behind the buildings may require additional parking to be accommodated in the overall site design behind the potential structure.
- 3 Corner of East Elm and Marshall: On either side of East Elm and Marshall Street intersection within the 100 block, corner properties may have opportunities for additional development. Currently they are being used for display of tractor and farm supply on the South and a vacant building to the north. Determine the space requirements for the existing business, and work with property owners to encourage private development.
- 4 Property Redevelopment: The City of Graham should work with the property owner of two banks, SunTrust and BB&T, on the properties at 220 or 236 Main Street. The City should assist with the redevelopment of these key gateway properties.

Conceptual Park Design

Small urban parks help create a more inclusive downtown, providing opportunities for exploration and reprieve from busy streets. An opportunity should be considered to redesign of Sesquicentennial Park in the Court Square. Optional water features providing audible and visual interest could flank the building walls, shade trees are centered over ample café seating, a central water feature provides a sense of enclosure, and a pergola with swings demarcates a connection to a parking lot behind.



Concept Park Perspective and Inspiration Imagery



Parking

Parking is a critical component of providing access to downtown's amenities, and accessible parking should be provided for users of each amenity or business. Convenient parking for Downtown Graham can be satisfied in a variety of ways and has been considered carefully in this plan. Successful parking programs that support vibrant downtowns must be balanced; parking stalls and lots must provide enough capacity to meet the demand for parking, but not exceed demand so much that it degrades city character or perpetuates unsafe conditions in the public realm. Parking design, location, and quantity should be integrated into the streetscape and public realm without dominating these spaces. The recommended concepts for Downtown Graham may attract several new users, both locally and regionally, and adequate parking should be ready for new visitors. Finally, parking lots and on-street stalls should also be easy to find, accessible, and safe for people driving, walking, and bicycling.

Using this approach to parking programming and design, the Graham Downtown Master Plan gives an overview of existing parking capacities, provides recommendations for new parking structures and location, and details changes to parking based on the recommendations for downtown.

PARKING INVENTORY

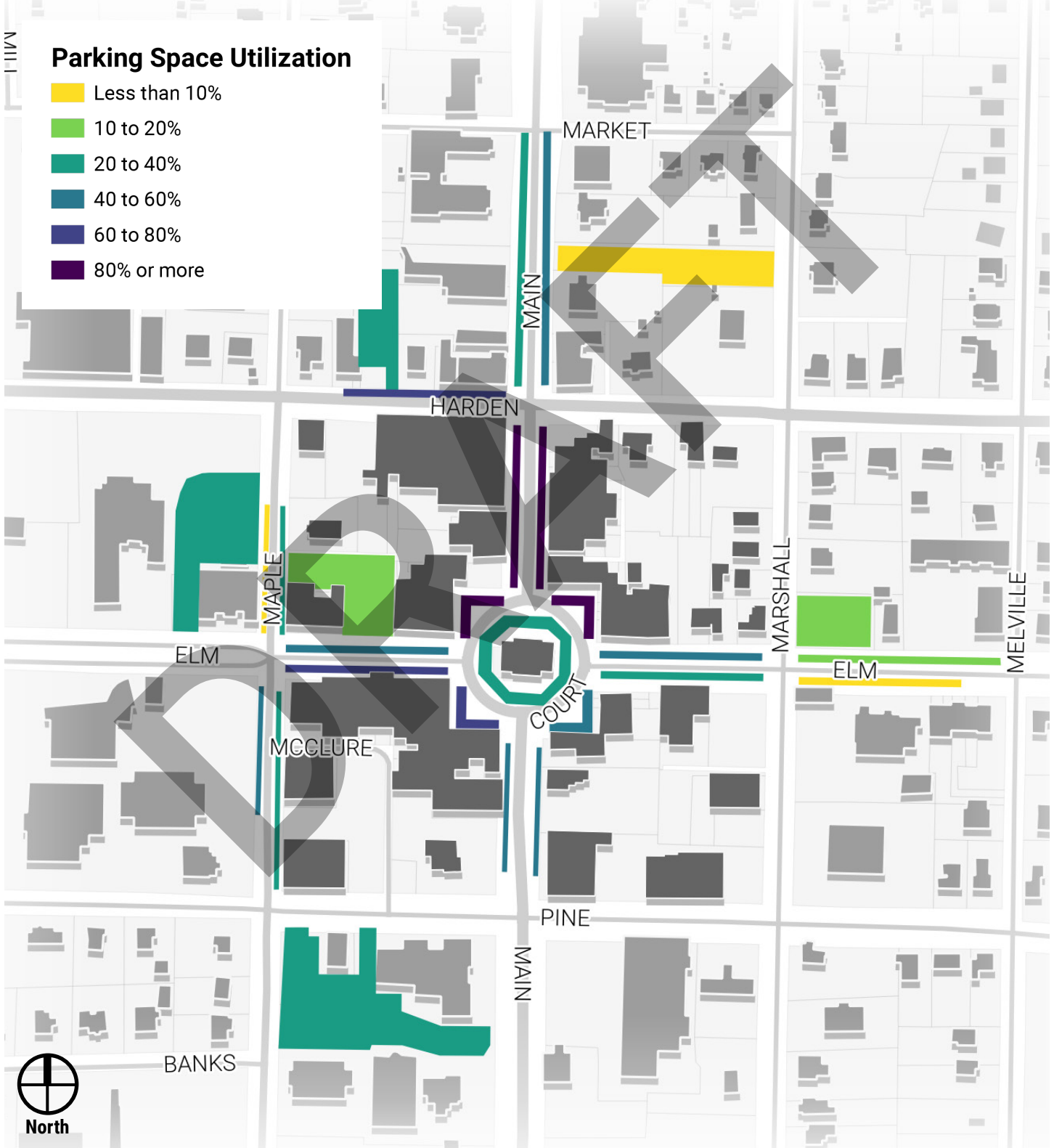
As a part of both the discovery workshop and the design workshop, the project team created a detailed inventory of existing parking in downtown. Today, there are over 600 parking spaces that provide convenient access to Downtown Graham. The map below shows parking locations within a quarter-mile from Court Square.

Existing Public Parking Locations



An existing parking study conducted by the City of Graham staff found that there is presently more parking availability than demand for parking in downtown. The study showed that average parking utilization for all of the parking in and around Downtown Graham is below 40% as shown in the map below, and that even during peak hours, the maximum parking utilization is below 55%. With the revitalization of Court Square and the approaches to downtown, parking utilization is likely to increase and may warrant additional parking for patrons of downtown.

Existing Public Parking Space Utilization



PARKING RECOMMENDATIONS

The proposed design for Downtown Graham prioritizes the public realm for pedestrians through the creation of gathering spaces, wide sidewalks, and storefront activity areas. Through a people-focused approach to downtown, the City of Graham and the community is investing in a future of vibrancy and growth for years to come. High-quality gathering spaces for people intentionally maximize pedestrian space and alternative transportation accommodations while ensuring enough capacity for vehicular parking and travel. The following describes recommendations for parking changes in and around Downtown Graham, beginning with restoring the historic alleyways. These recommendations should be considered comprehensively; not forgetting the existing inventory of public parking available in close proximity along the streets and public lots. Traditional head-in angled parking is preferred by the City of Graham along with parallel parking.

Restore Historic Alleys

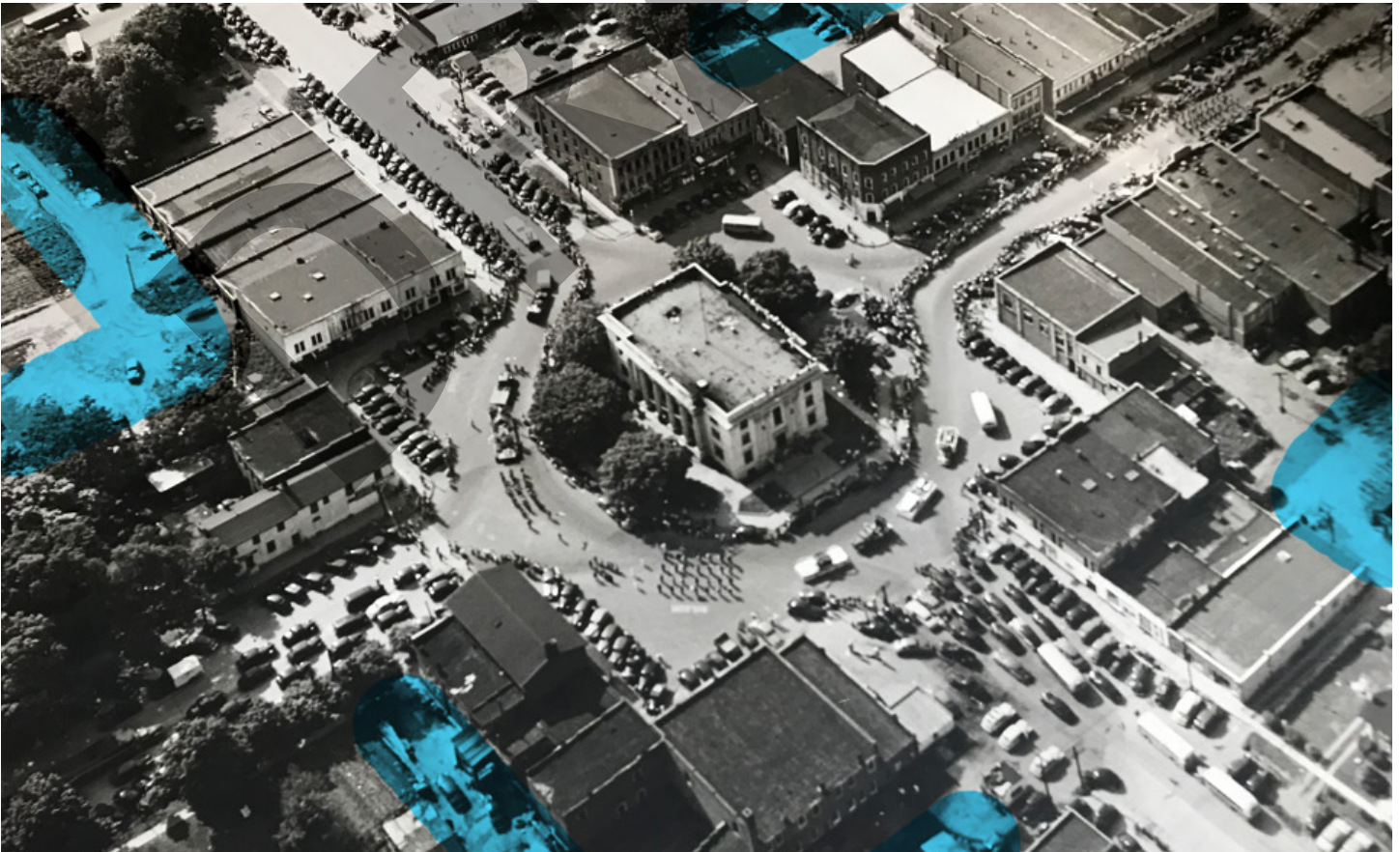
Downtown Graham has a historical character that has been preserved in a manner that adds to the sense of place for the community. Remnants of historical alleys still exist within downtown but have been repurposed or

reduced over time. A key recommendation of this plan is the assessment of the opportunity to restore the historical alleys in the heart of Downtown Graham to serve back-of-house functions for deliveries, trash and recycling pick-up, and additional parking.

Reestablishment of alleys should occur where possible through public-private partnerships with the cooperation of existing property owners.

Alleys should be retrofitted to ensure vehicle circulation and delivery truck movements are accommodated. The City should explore the options for alley alignments similar to the historic alleys that Graham developed earlier in the community's history.

Existing parking utilization in Downtown Graham and surrounding lots suggests that relocating existing parking would still serve residents and visitors in Graham. However, changes to the streetscapes impact more than just the parking around the Court Square but also loading and unloading of delivery vehicles for several businesses. Alley restoration does not neglect these functions but establishes clear loading and unloading zones for business that front the 100 blocks of Main Street and Elm Street.



Additional Parking and Capacity Assessments

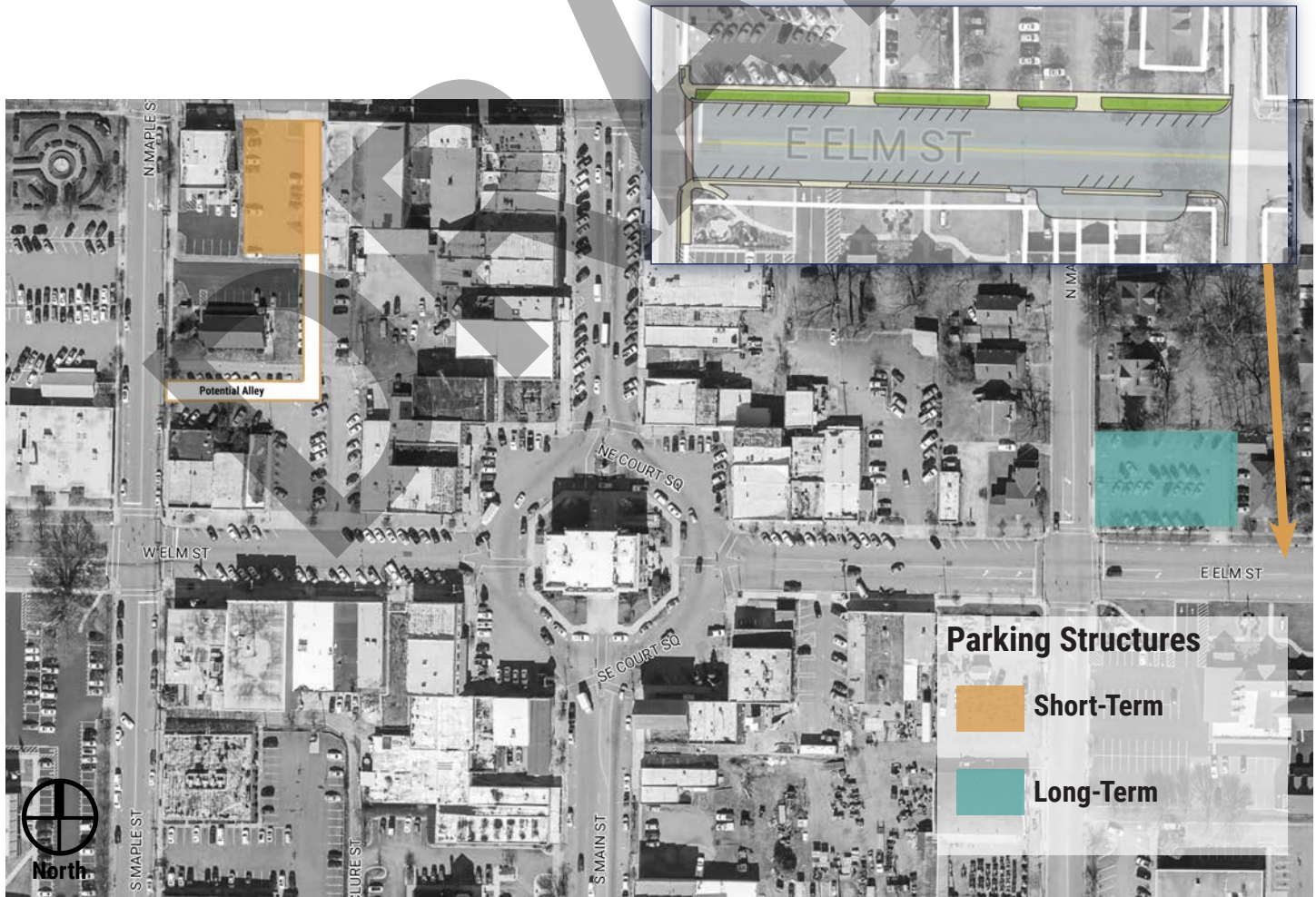
The City of Graham Downtown Master Plan proposes significant changes to downtown that will ultimately attract more visitors, commerce, and trips to downtown. As Downtown Graham gains momentum, routine evaluation of the parking utilization should be conducted by City staff to track parking needs and changes in parking demand.

Short-term, a parking structure should be considered along Harden Street to accommodate 50 or more new parking stalls and to provide an overall net gain in parking in the core of Downtown Graham. All parking structures should fit the character of downtown and should use high-quality materials and focus on a street-facing façade that is attractive and could even provide ground floor retail. An alley should also be considered adjacent to the parking structure to provide additional circulation and opportunities for back-of-house functions. Both the proposed parking structure and alley in this location are on property owned and maintained by either the City of Graham or Alamance County. Additional

parking capacity is also available through re-striping existing on-street stalls along East Elm Street between Marshall and Melville street.

Long-term, there may be a need for an additional parking structure to accommodate daily users of Downtown Graham. Daily parking utilization throughout the downtown area should be consistently above 80-85% before the need for an additional parking structure is evaluated. Redeveloping an existing public surface parking lot could provide a net gain of 50 or more parking stalls.

Success for downtown is contingent upon planning for the future. Some parking demand may be addressed by future private development due to the market pressures. The map below illustrates the short-term parking structure recommended along with the proposed alley on City/County property. Future parking structure location and a graphic illustrating a potential re-stripe of East Elm Street are also included.



Court Square and 100 Blocks

Reimagining Downtown Graham increases the amount of useable public space in the Court Square and surrounding 100 blocks of Elm Street and Main Street. The proposed concept for Downtown Graham provides options of parking in Court Square with the opportunity for up to 40 parking stalls. Additionally, angled parking on both North and South Main Street blocks is recommended. Parking along East/West Elm Street is envisioned to be parallel parking to increase pedestrian space on each approach to Court Square.

Maintaining adequate parking for Downtown Graham is essential to provide balance for residents, visitors, and business owners. Current utilization of parking in Downtown Graham provides some flexibility with regards to parking; however, the vision for revitalization is expected to increase demand for access to Downtown Graham. With this in mind, the project team has identified several areas where additional parking may be gained in and around downtown including through the restoration of alleys (through public/private partnerships), and re-striping existing public parking areas. Parking stalls that are ADA compliant should be placed toward the middle of each block to ensure easy access and short trips for residents and visitors with disabilities.



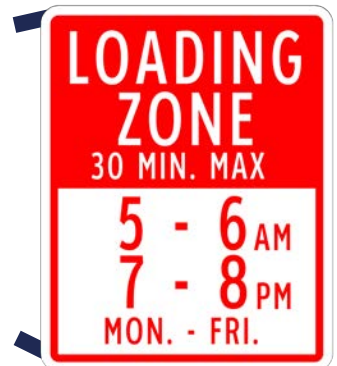
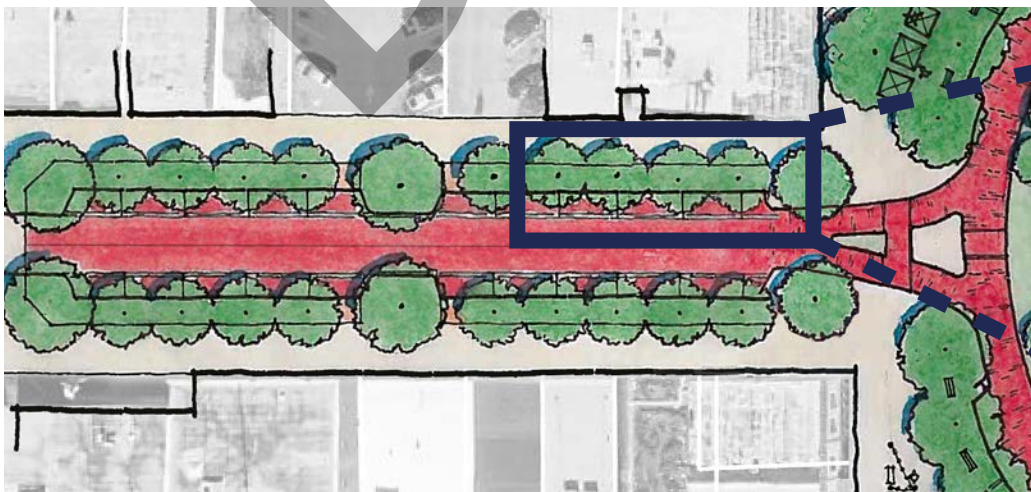
Mention was made that there still is a hope alive in the commission that parking on the Court Square be discontinued, not only to upgrade the looks of the square but to cut down on the danger to pedestrians and passing cars.

Graham Beautification Commission (1979)



Loading Zones

Merchandise, supplies, and food are all essential for retail establishments to succeed. Deliveries must be made quickly and efficiently on a routine basis. In a downtown, these functions are better served through alleyways with rear access to businesses. While Graham explores the possibility to reestablish its historic alleys, loading zones should be considered. A standard sign pictured below will mark designated spaces and hours for loading on Elm Street and Main Street during a one-hour window, both am and pm, at the consensus of Graham’s merchants. Delivery companies across the country are accustomed to this approach and are willing to accommodate needs. The loading zones are for large tractor trailer style delivery vehicles. Smaller parcel delivery will simply park in a parking spot or on a surface lot nearby to deliver parcels.



PROGRAM RECOMMENDATIONS

Signage and Wayfinding

Throughout the public engagement process, several participants said that they were not aware of nearby public parking lots outside of on-street parking spots on Main Street and Court Square despite existing parking wayfinding signage. This is also reflected in the City of Graham's parking study, which found that the most heavily utilized spaces are those on Main Street and within the Court Square. Existing public parking lots are in close proximity to Court Square and many are routinely underutilized or even empty.

This likely indicates a need for a more robust signage and wayfinding program. Parking lots and on-street parking should be easy to find from anywhere in downtown, and signage should be consistent and clear for residents or first-time visitors alike. Signage connecting parking areas to downtown should accomplish three goals:

- Signs should be clearly visible signs at all entrances and exits of parking areas, and they should point users towards major destinations;
- If there are multiple routes to a destination, signage should help users easily choose routes into downtown Graham from each of the parking location; and
- Parking signage must be visually pleasing and distinct to Graham, contributing to the city's overall sense of place.

Pedestrian Wayfinding in Omaha, NE



Activated Alleys

Potential alleys in Downtown Graham present an opportunity to not only serve the back-of-house, but to give people a sense of discovery as they approach Court Square. Walkways into downtown, including alleys and sidewalks from nearby parking lots, should be well lit, clearly defined, and inviting. The alleyway additions in the proposed concept provide an ideal opportunity to invite people to walk towards downtown from other parking spaces. Activated alleyways use street art, festive lighting, and high-quality landscaping to serve as destinations in and of themselves. They can provide a fun and engaging space for pedestrians while still maintaining functionality for deliveries and services when needed.

Outdoor Dining in Alleys



DRAFT

ONEIDA

ELM

MCBRIDE

HOLT

BANK



DRAFT 4

STORMWATER + UTILITIES

Existing Conditions
Proposed Treatment

MCADEN



STORMWATER + UTILITIES

Revitalization of Downtown Graham is not solely dependent on a vibrant streetscape or vertical construction. At its very foundation is the infrastructure needed to support growth. Stormwater and utilities must adequately serve the current infrastructure but allow for future growth and demand. Graham has experienced inadequacies in their system first hand through flooding in Court Square, sewer capacity issues with new construction, and system failure with age. Without adequate utilities in place development cannot and will not occur. As capital improvement projects begin these systems must be overhauled and included as part of every project.

This chapter will identify the existing conditions within Downtown Graham. It will provide both short- and long-term solutions for infrastructure needs that are traditional and non-traditional using today's best practices. Recommendations will prepare Graham for the next 100 years and will help create a more self-sustaining and adaptable system over time.

Chapter Components

Existing Conditions:

Understanding existing utility conditions starts with a good survey, gis, research, and analysis. An in-depth look at downtown's current infrastructure shows numerous issues and inadequacy.

Proposed Treatment:

Industry standards and best practices are highlighted to provide Graham with a menu of treatment options both above and below ground.

Existing Conditions

Over time, parts of Downtown Graham's network of stormwater and utility facilities have typically been replaced due to associated roadway work or for critical repairs. The nature of these improvements have created a system with uneven capacity and an incongruous layout. This project presents a great opportunity to lay the groundwork for improvements that will allow for easier maintenance and more regular repair cycles while also providing relief to the downstream conditions for a greater range of storm events.

A stormwater network is commonly comprised of collection and conveyance facilities such as inlets, manholes, and pipes that collect stormwater runoff from the roadway surface and corridors and then convey it to flood control and/or stormwater quality facilities or directly to local outfalls. Outfalls can be minor or major drainageways such as streams, creeks, wetlands, or rivers. For stormwater networks along roadways, stormwater is collected by inlets and conveyed in pipes until outfall. The main conveyance pipes are typically called trunk lines. This conveyance and stormwater network can also be described as hydraulics. Hydrology encompasses stormwater catchments or the area draining to a source such as inlet, water quality feature, or detention facility. A minor catchment describes the area draining to a specific inlet and the major catchment, a specific outfall. Detention facilities are used to reduce peak flows, defined for this project by the 10-year and 100-year design storms, and are placed at points along or at the end of the conveyance facilities.

EXISTING CATCHMENTS

Two main catchments in Downtown Graham run to the east and to the west of Court Square. Each of the catchments around Court Square primarily include impervious area and roof drainage from adjacent buildings.

Stormwater from the western watershed makes its way to the stormwater trunk line starting on West Elm Street. In general, the overland flow paths follow the stormwater network and are consistent with surface grading. The system outlets into the Bowden Branch which merges into the Little Alamance Creek before connecting to the Haw River.

There are three minor catchments to the east that ultimately outlet into Town Branch, a tributary to the

Haw River. Flows from the southeast and Northeast catchments ultimately converge in the ditch just north of the intersection of East Harden Street and Oakwood Drive, near the Linwood Cemetery. Runoff along North and South Main Street is kept to a minimum with a system of inlets and trunk lines. The Main Street catchments are divided between roadway high points and collected runoff flows to the east just north of East Harden Street and at the intersection with East Pine St.

The Haw River watershed, including the Bowden Branch and Town Branch Creeks, is classified as Water Supply V (WS-V) and Nutrient Sensitive Water (NSW). NSW rivers need nutrient management due to excessive microscopic or macroscopic vegetation.

EXISTING CONVEYANCE AND WATER QUALITY

Currently there are limited water quality facilities in Graham, however some systems outlet into grass channels that may offer some treatment during small events. Most of the pipe network at or adjacent to Court Square is undersized for the current levels of impervious surface that discharges into the system.

Graham needs better drainage and collection in between buildings, in alleys, and at intersections surrounding Court Square. There are corners where, after storm events, water forms surface ponding or pools. The depth of the systems that collect drainage from the square are typically between 4- and 5-foot deep. There is an existing valley gutter collecting parking lot and roof runoff in the northeast quadrant of the square. The most common collection structures are grated inlets. Many are dated or broken and should be replaced with more pedestrian-friendly grates.

Along Main Street there are systems that collect water in inlets and into trunk lines that flow east to the Haw River tributaries. The main branches from Main Street are at the public parking lot 300 feet north of the Harden Street intersection and at the East Pine Street intersection. At the time of this report NCDOT is currently installing additional catch basins in Court Square to tie into the main branch located at the Pine Street intersection

UTILITIES

There are several utilities serving Court Square that are

known to need an upgrade if they are to comply with current standards. There are also abandoned utilities that need removal, such as the water main surrounding the square. Alternately, it may be more economical to plug and fill abandoned lines with flowable fill. With the existing utility layout, there is an opportunity during roadway changes to upgrade sewer and water services to a best practice layout and to remove obsolete utilities. Any proposed work adjacent to existing utilities will require examining existing conditions and service agreements with the City of Graham. In addition to upgrade needs, power and telecom lines are typically overhead once outside the downtown area. In some cases, roadway work that impacts poles may require incorporating the utility and funding its undergrounding. Utilities should be upgraded once roadway design is initiated to avoid future patchwork repairs.

The following are the known utility providers in the project vicinity:

Water & Sewer: City of Graham

Telecommunication: AT&T, Century Link, and Time Warner Cable

Gas: Piedmont Natural Gas and Public Service Company of NC

Power: Duke Energy

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Proposed Treatment

The goal for stormwater in the City is to collect stormwater and treat all areas where the City makes improvements, however that will likely be a step-by-step process. In the interim, the detention of stormwater is as essential as collecting and treating it due to the undersized pipe network downstream from the project area.

Water quality and detention facilities shall meet North Carolina Department of Environmental Quality (NC DEQ) standards for post-construction as well as the City of Graham's Stormwater Ordinance and permit requirements. The current ordinance describes many parameters for water quality and quantity facilities that shall be met or exceeded. It also describes measures for Nutrient Sensitive Waters, water quality, and other stormwater system requirements. If needed, the ordinance should be amended or adapted upon installation of the proposed design features.

GREEN INFRASTRUCTURE

Green infrastructure uses vegetation, soils, and other elements and practices to reduce runoff and treat stormwater at its source. It seeks to mimic the response of the natural catchment and its surfaces. With management and control structures, green infrastructure can drain and treat surface water in a more sustainable approach.

The vision for sustainable stormwater management in Graham is to evolve the green stormwater treatment over time to fit with the unique characteristics of the region. Typically for Graham the facility type should be a bioretention planter or swale with additional opportunities for tree well treatment devices or manufactured devices in very constrained situations. Green infrastructure, also known as structural Best Management Practices (BMPs), will help infiltrate stormwater, reducing runoff and alleviating pressure on a failing system. With each improvement, green infrastructure can be an increasingly integral piece to improving aesthetics and water quality while providing detention for minor storm events. Substituting a traditional pipe and inlet system with a green infrastructure facility provides an opportunity for both natural and artificial stormwater runoff and peak flow reduction.

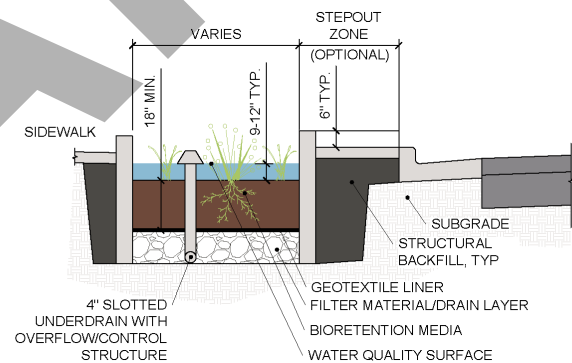
Green infrastructure will serve to stimulate plant diversity, provide shade, and help create a visually pleasing Court Square and Main Street corridor. Rain water from nearby roofs will be directed into bio-retention planters and tree wells. To ensure a low-maintenance landscape, native planting will be incorporated into the design of each planted facility. Green infrastructure can be adapted to a

wide range of soil types, catchment pavement types, and stormwater system connection depths.

Bioretention Planters

Bioretention planters are typically placed between the street and building or right-of-way. The planters are located within the furnishing zone and face the street, with curb openings serving as inlets, and can be integrated into a variety of adjacent street and pavement types. Runoff is collected, spreads across the planting media, infiltrates into the media, and flows into an underdrain that connects to a pipe network. The treatment process includes filtration, plant uptake, heavy metal reduction, and absorption. Depending on the existing soil type, filtration can have a large impact on performance and need for an underdrain. The image below shows a typical planter section that allows flexibility and can function in a variety of settings around downtown.

Typical Bioretention Planter Section



To allow for planting of trees along with shrubs, grasses, and other native groundcover, the minimum inside planting media width should be five feet. The proposed planters should maximize the width to allow for flexibility and adaptability of the planting over time. Where a planter is adjacent to parking, a step-out zone should be provided where space permits.

For situations like the existing valley gutter at Court Square, a minimum width planter (sometimes referred to as a green gutter), can be retrofitted in place to provide treatment. The minimum width for a green gutter is three feet for maintenance and planting feasibility.

Tree Wells

Tree wells have emerged as a popular device for integrating treatment and detention with a maintainable

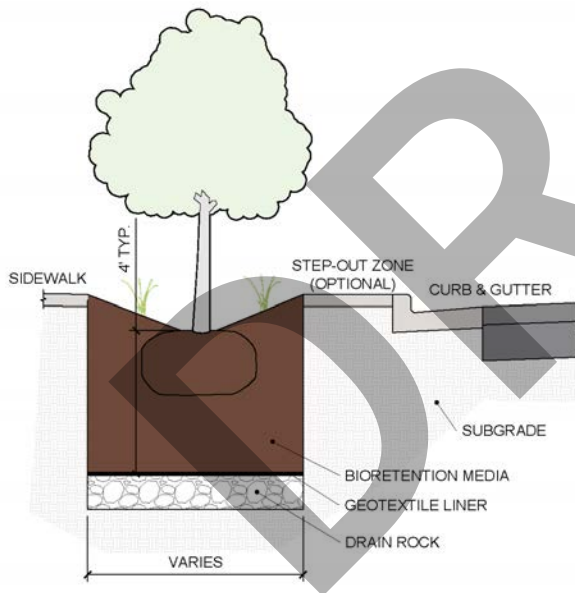
tree area. They can be successful in even the most urban settings, as they provide great flexibility in size, efficiency, and connection to existing stormwater systems. Since tree placement will be a significant part of the proposed downtown design, tree wells can be a beneficial utility for meeting the City's stormwater needs.

A tree well is a type of bioretention facility that provides treatment from street or sidewalk runoff. Runoff typically enters from a curb opening or trench drain inlet and flows through filter material before being collected in a perforated underdrain. Tree wells are configured to allow infiltration into the existing ground while still prioritizing uptake by the tree roots.

Since the treatment is contained underground, tree wells can be used in constrained locations or where a paved surface is preferred. They work well in plaza spaces or other areas with a lot of pedestrian activity because they keep the treatment function hidden from view.

The image below shows a typical cross section and components of a tree well:

Typical Tree Well Section



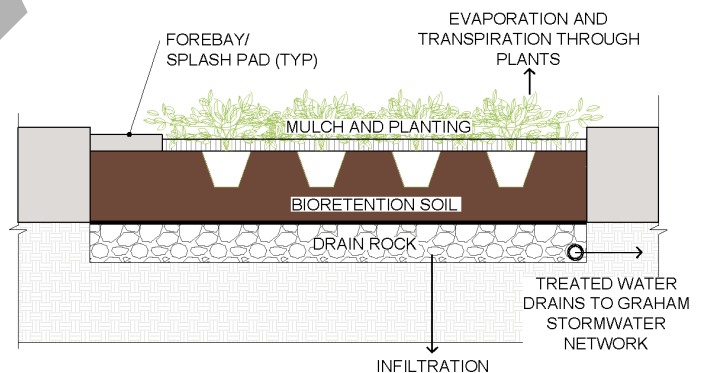
The City can either develop its own tree well design or use a proprietary device. Selecting a common design or device will allow for future ease of construction and maintenance needs. With either option, multiple trees can be included in each tree well, as can added space for detention. If using a proprietary tree well, the design should meet recommendations contained in the NCDOT Stormwater Manual. Common proprietary tree wells include Deeproot's Silva Cell and Imbrium Systems' Filterra Biofiltration system.

MANUFACTURED TREATMENT DEVICES

Manufactured devices are being used around the country in a variety of contexts, and the current best practice is to use them where there are spatial constraints and ongoing maintenance isn't a major concern. The central benefit to manufactured devices is that they are contained underground while offering good treatment efficiency. They can be selected based on site-specific issues or to target certain pollutants. From a low-impact development standpoint, these devices are a secondary option for this project. In addition, some devices can have relatively high initial and ongoing maintenance costs compared to more natural treatment features.

Only devices that are verified by NC DEQ should be installed. Contech's StormFilter is an example of a certified manufactured device that works in place of street-side inlets. A StormFilter is a good example of a device that can be used around the City and that will work in most roadway contexts. Imbrium Systems' Stormceptor is an example of a device that works in place of a manhole and is a separator designed to remove total suspended solids, oils, heavy metals, and nutrients. A typical Stormceptor is represented in the following figure.

Typical Stormceptor Section



Source: Imbrium Systems – Stormceptor

NCDOT's Stormwater Design Manual also specifies minimum design criteria and recommendations for new stormwater technologies. Any devices utilized should meet or exceed these recommendations.

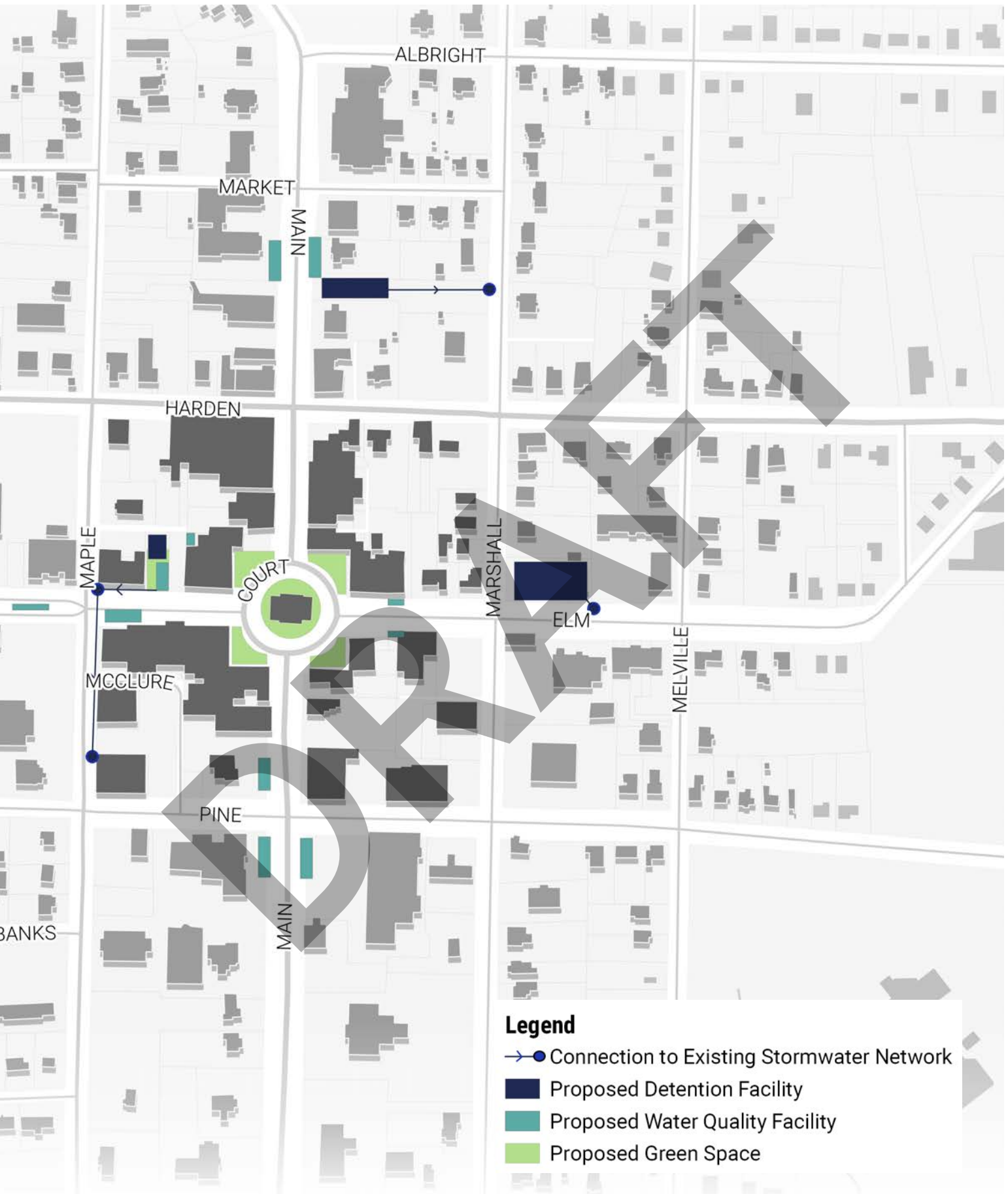
UNDERGROUND DETENTION

With the proposed roadway layout and limited public open space surrounding Court Square, underground detention will be the best way to detain water and reduce the load on the existing drainage system. The proposed roadway design will reduce peak flow by reducing the impervious area. Providing detention will bring the added benefit of relieving the downstream system and reducing the need to pay the high cost of upsizing long drainage trunk lines in the future.

Multiple city-owned parking lots located near the existing conveyance from the square have adequate space to provide a significant delay in peak discharge. With the layout of the existing catchments, multiple facilities will be required to have the maximum impact. The proposed roadway design will allow implementation of new conveyance and detention facilities without introducing interbasin transfer or significant City system stormwater transfer issues.

Please note: Other potential locations for greenspace should also be evaluated and created in cooperation with downtown property owners.



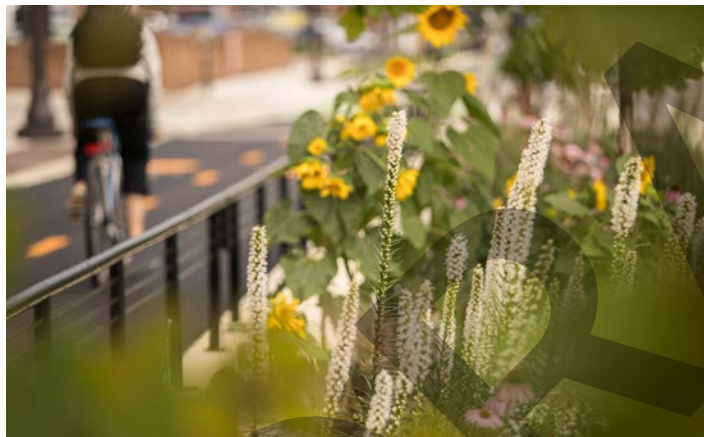


STORMWATER + UTILITIES RECOMMENDATIONS

The proposed improvements to the water quality and storage network can be phased in line with the overall project timeline described in Chapter Five.

Water Quality Facility Recommendations

An incremental approach is recommended for stormwater quality improvements. At each location of roadway reconstruction, water quality facilities should be placed and included in the design for maximum treatment benefit. Where space and budget permit, alternative facility details should be developed to increase detention for large storm events.



Recently completed green infrastructure in Saint Paul designed by Toole Design

Bioretention planters are proposed at or near low points along Main Street and should connect to the main east-west conveyance systems. The planters should be sized

to treat the water quality event for the impervious surface of the proposed roadway layout to the maximum extent possible. Tree wells are proposed in and around the square where there is reduced ground space for planters.

In future phases of the proposed projects where planters or tree wells aren't feasible, the City should look for opportunities to bank the equivalent impervious area for treatment. Ideally the treatment could occur along the same conveyance system and serve the same function upstream or downstream. There may be circumstances where bioretention is better served at another location with more available space.

A short term, low-cost solution, is to simplify the planter design to closely resemble a grassed swale. If the facility can be arranged in a way that minimizes concrete walls and simplifies the planting to native grasses overall costs will be greatly reduced. The challenge for the low-cost facility is finding adequate landscape space to provide effective treatment. Ideally, trees will be included in the swale. The swale design should follow NC DEQ guidance for a Treatment Swale and have flexibility to be upgraded to a planter in the future.

Stormwater Detention Recommendations

Typically, an analysis of pre- vs post-construction runoff would provide a starting guideline for goals. For this project, because the catchment areas are made up primarily of impervious surfaces, the goal is to provide the maximum water detention feasible once roadway design is initiated. Water quality facilities should be included in detention calculations for a fully representative model and should be part of the overall strategy to reduce peak runoff. Graham's Stormwater Ordinance specifies post construction treatment requirements but measures above those are needed for the future projects to provide a benefit.

The OPCC could vary based on location but may be similar of site of comparable size and scale.

There are three proposed locations for detention facilities; each is a City-owned parking lot near a conveyance system collecting runoff from Main Street and surrounding Court Square.

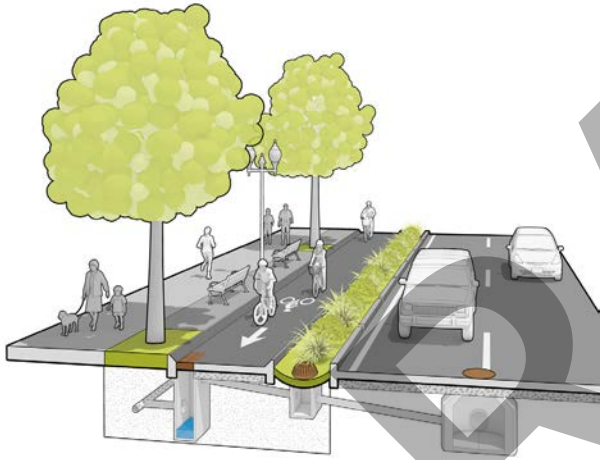
Two options are proposed for the design of the facilities:

- 1) Non-Proprietary
- 2) Proprietary

The non-proprietary option involves developing a project-specific design using pipes aligned in a row, maximizing the funds available. This design could be built up over time and maintained by City staff. The proprietary system is a device such as a StormTech Chamber, which may require more physical infrastructure but will likely yield more controlled results and better performance. Both systems can be designed to include infiltration and sediment control, and to control the water quality volume and initial surface runoff.

The proposed detention facility at Main Street should be constructed when the roadway is re-constructed in line with the overall project phasing. Priority between the two locations collecting runoff around the square should go toward maximizing the runoff reduction impact while considering existing capacities downstream.

Example of a green infrastructure facility



The proposed stormwater model should provide a comprehensive analysis of water quality facilities, paving surfaces, and optimization of runoff routing.

Stormwater Network Upgrades

While stormwater network upgrades will occur along sections of future roadway work, there are some drainage lines that need to be replaced to allow the proposed detention and water quality device locations. Stormwater pipes downstream of identified detention and treatment should be replaced if it will assist with the facilities function.

The line along Maple Street between Elm Street and Pine Street must be lowered at Elm Street to allow depth in both detention and water quality facilities in the City-owned parking lot. There is adequate grade difference to allow the drop with approximately 400-ft of new pipe

required. Making this upgrade in an early phase will allow flexibility to make future system improvements and variation in treatment facility types.

Maintenance Requirements

Non-traditional stormwater systems are not inexpensive and require additional maintenance from qualified personnel. It is essential that water quality and detention facilities are maintainable by City forces and financially sustainable as more are added. Installing a more uniform and consistent system will help ease maintenance needs, especially from a planting perspective. Green infrastructure should be included in each phase to meet water quality requirements to the maximum extent possible while still being viable long-term solutions. The maintenance of these facilities will be crucial to the overall performance of each element. Additional trained personnel will be needed to ensure each facility continues to function after the implementation is complete.

If needed, the City should amend the stormwater ordinance to help with long-term maintenance of the water quality and detention facilities designed as part of future roadway works. Consideration of more stringent water quality and quantity controls will help reduce the amount of future runoff.

Proposed Treatment (Water quality and quantity)

- Underground detention areas identified
- Bioretention or other water quality areas to be explored (including facility types)
- Reduction in impervious area along corridors
- Future development policy to be explored

Implementation and Phasing

- Phased so that the City of Graham can implement in small stages
- Short-term lower cost solutions (water quality & quantity)
- Long-Term system wide solutions (water quality & quantity)
- Plan for next 100 years with high-level costs for each stage

DRAFT

ONEIDA

ELM

MCBRIDE

HOLT

BANK

HARDEN

COURT

MAPLE

MARSHALL

MELVILLE

PINE

DRAFTS

PHASING + IMPLEMENTATION

Phasing
Implementation
Funding

MCADEN



Sutton's

114

NO BICYCLES
OR
SKATEBOARDS
ON SIDEWALK

NO PARKING
IN
DRIVEWAY

Sutton's

Sutton's

HOURS

GRUBH

PHASING + IMPLEMENTATION

A plan that is not ground in truth is a plan that will sit on a shelf. The Downtown Graham Master Plan will not be realized overnight. Careful planning, negotiating, design, funding, and construction can take several years to materialize. Understanding this, attention must be given to a succinct and well thought out phasing and implementation plan.

This chapter seeks to guide the City through the proposed phasing, implementation, and funding process, outlining a clear path forward to a lasting legacy in downtown.

Chapter Components

Phasing:

The downtown master plan will be implemented over the course of several years. Logical sequence of construction is established to guide Graham over the next 10 years.

Implementation:

How does Graham move forward with the recommendations presented throughout the preceding plan? Strategies and solutions to implement a successful multimodal network, provide and inviting streetscape and public space, and establish sufficient stormwater and utilities will be discussed.

Funding:

Funding is the lifeblood of implementation. Identification of potential funding mechanisms is critical.

Phasing

Implementation of a downtown masterplan without a clear path forward can feel like a daunting task. A phasing plan establishes a clear path forward breaking up projects into manageable and achievable projects. The diagram on the following pages represents prioritized projects and the recommended sequence of implementation based on community feedback and existing capital improvement projects.

PHASING

01

NOW

MULTIMODAL NETWORK

Policies and Programs

Data Collection

Complete Sidewalks along Maple and Marshall (Elm to Gilbreath)

W. Market Street - Neighborhood Bikeway (W. Elm to Maple)

W. Market Street - Slow Street (Maple St. to N. Main)

Pine Street Bike Lanes

Oneida Street - Neighborhood Bikeway (Market St. to W. Elm)

N. Main Street Bike Lanes

McAden Street Bike Lanes

Branding/Wayfinding

STREETSCAPES & PUBLIC SPACES

Alternative Truck Route

Court Square - Pilot Project

Court Square - Reconstruction

Elm Street (100 Blocks)

Main Street - South Main (McAden to Court Square)

Main Street - North Main (Court Square to Albright)

Parking

Parking Structure

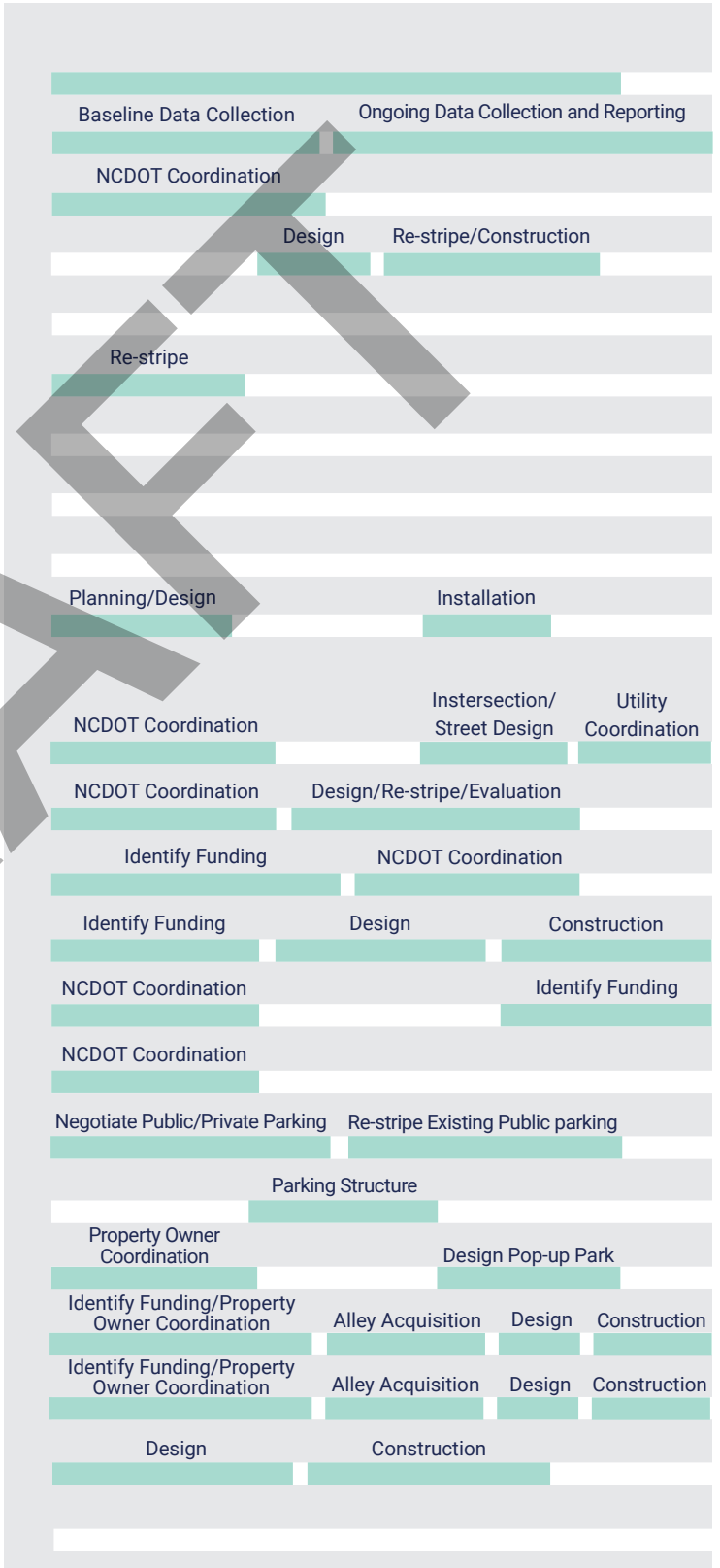
Green Space/Adaptive Reuse

Restore Historic Alleys - NW Block

Restore Historic Alleys - SW Block

Eastern Gateway - E. Harden/E. Elm (already in design)

Western Gateway - W. Harden/W. Elm

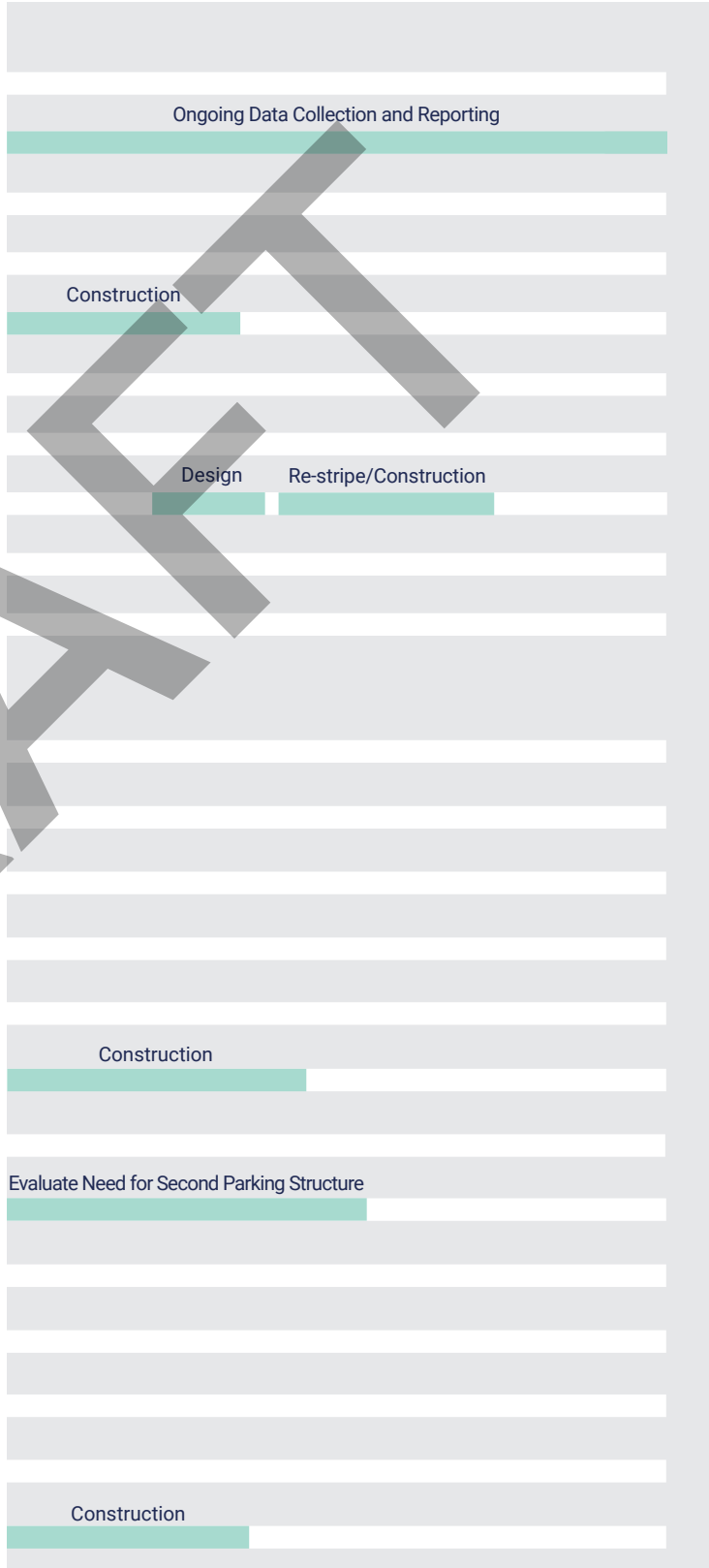
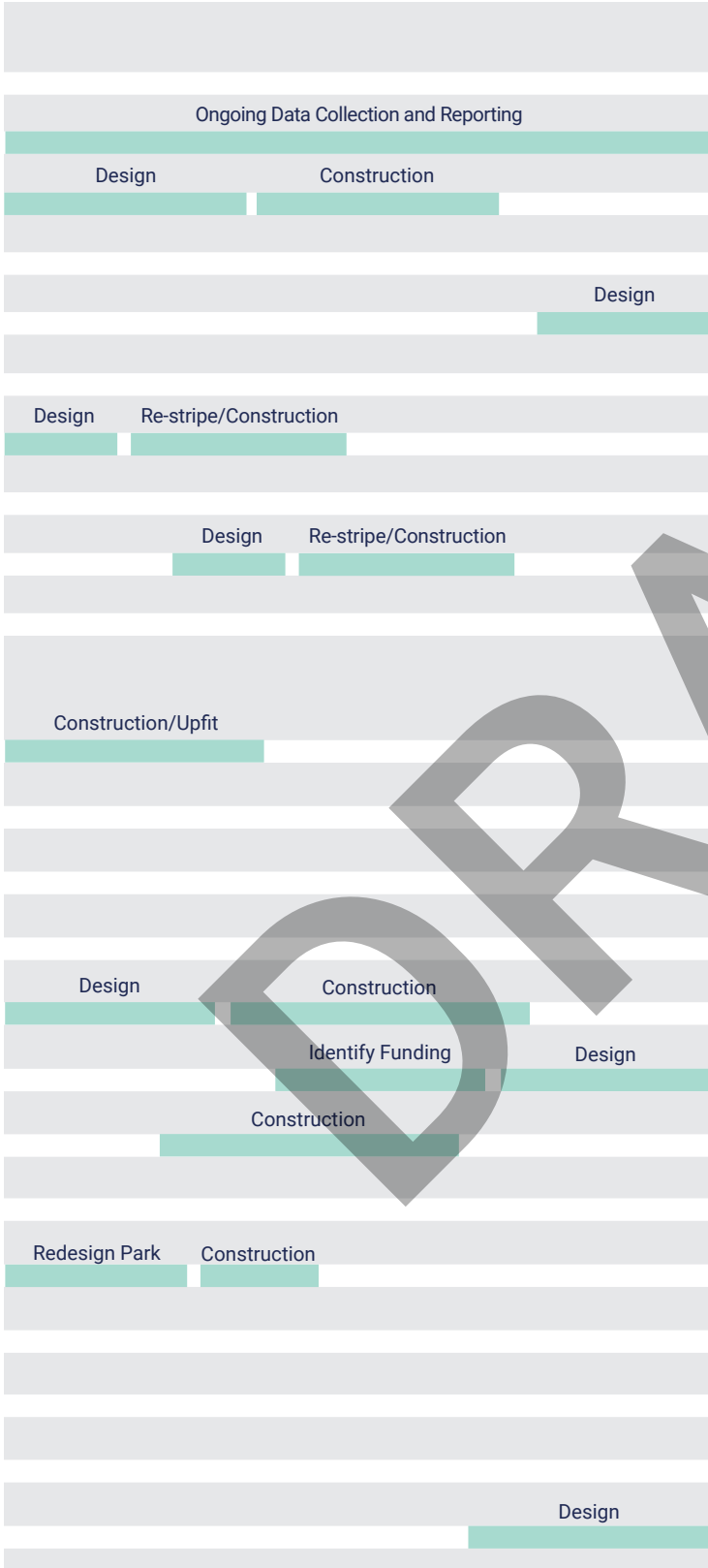


02

3-5 YEARS

03

6-10 YEARS



Implementation

The visioning process for Downtown Graham has brought the community closer together than ever before. There is excitement and a hint of skepticism across the community. Will the City move forward with recommendations? The plan itself is just the beginning of the process and stirs up momentum for change. The momentum gained during the process must be continued. Now that a clear vision and Concept Plan has been established, complete with an understanding of community desires and guidance on key design features, it is time to advance the plan to reality.

Recommendations go beyond the physical design of the public realm and include program and policy changes that can increase safety and health for people enjoying Downtown Graham.

Graham should start with the following:

Consistency Statement:

-Ensure all future actions and funding lead to a better Graham and point back to the 2019 Downtown Master Plan.

Complete Streets Policy:

-Language exists in the current development ordinance but does not carry enough weight and leaves too much up for interpretation.

-Provide guidance and assistance with NCDOT projects that impact downtown Graham.

Evaluate Potential Return on Investment:

-Return on investment is not simply economics, while this is a valuable aspect several other factors contribute to the return:

- Health
- Quality of Life
- Environment
- Safety

-Graham must ask itself when every project is evaluated for inclusion in the fiscal year budget, is what we are about to approve going to contribute to the potential return on investment established in this plan?

MULTIMODAL NETWORK

As Graham grows its bicycle and pedestrian network, it will become a more vibrant and more accessible community. Implementing multimodal facilities can be challenging but provides mobility options for residents and visitors. While not exhaustive, the following list outlines some of the most important steps for creating a successful network of bicycle and pedestrian facilities:

01. Create policies that support the implementation of bicycle and pedestrian infrastructure and the growth of active transportation culture

Sidewalk gap programs, bicycle/pedestrian counting programs, bicycle and pedestrian safety curriculums in schools, and Complete Street ordinances can create frameworks for successful implementation and use of a network for years to come.

02. Prioritize projects

While all projects are essential to creating a connected community in which to bicycle, walk, and wheel, some projects will create larger impacts. This could be because they connect more people to key destinations, or it could be because the community is particularly excited about certain projects. These projects should receive more immediate planning and implementation efforts. The City of Graham can identify which projects are the highest priority by creating a prioritization scoring mechanism and/or through public engagement.

03. Create metrics for success

Before implementation, Graham should have a clear picture of what “success” in its active transportation network looks like. Creating measurable goals and clearly defining desired outcomes will help staff identify priority projects and set a strategic direction.

04. Implement “low hanging fruit” projects

Some parts of the network may be easier to implement than others due to the project type, community support, or specific funding mechanisms. These projects should be identified and slated for implementation as soon as is feasible.

05. Identify funding mechanisms that are suitable for the project and community

Not all funding mechanisms work in all communities. The City of Graham should assess the feasibility of municipal bonds, impact fees, voter-approved sales taxes, state/federal funding grants such as BUILD grants, and partnerships with state organizations like the North Carolina DOT and Department of Environmental Quality.

06. Over time, add higher priority projects to the Capital Improvement Plan

As is appropriate, add the highest priority projects to the city’s capital improvement plan.

07. Identify community partners and champions

Some community members will be “champions” of infrastructure improvements that support better bicycling, walking, and wheeling. This could include local advocacy groups, community leaders, school students and teachers, and members of religious congregations. Additionally, NCDOT should be a partner throughout the process. The creation of a health advocacy committee to review proposals and make recommendations will help Council to make decisions and create budgets.

The Downtown Graham Master Plan identifies several recommendations that increase mobility options and expand the safety and connectivity for pedestrians and people that desire to ride a bicycle. The following provides additional detail on implementation for several recommendations that have been included in the phasing plan.

Policies and Programs

Transportation and streetscape recommendations are critical to establishing Downtown Graham as a vibrant and safe place for user; however, local policies and programs should be updated and developed to ensure consistency with the vision and goals of downtown. Policy updates often include local resolutions or revisions of development ordinances that can guide private development to contribute to the sense of place approaching the Court Square and throughout downtown. In addition to policies, programs should be considered to celebrate the changing public realm and encourage users to experience Downtown Graham in a new way. Programs for data-collection should also be considered and are described in the following section.

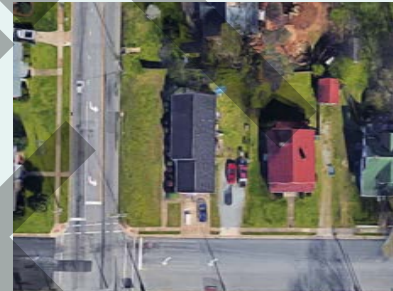
Project Phase: One

Policy and Program updates should consider:

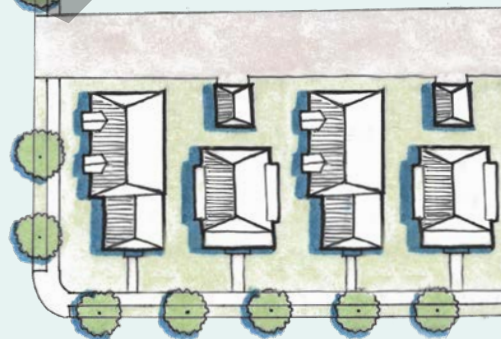
- Updated to development regulations and zoning ordinances that may include but are not limited to:
 - » Reducing the minimum lot size for commercial and residential properties within the downtown area to encourage more dense development.
 - » Establish build-to requirements for all development along the gateway corridors and within the Court Square. This may include a zero-foot setback for any new development in the Court Square or in the 100 blocks of Main Street or Elm Street.
 - » Update off-street parking regulations to require vehicular parking to be located behind buildings along Main Street and Elm Street. Consider parking maximums for downtown residences and design enhancements to reduce parking provided per site.
 - » Ensure that development ordinances require bicycle parking for commercial development for properties along Main Street and Elm Street.
- Bike to Work/school day - an annual event to encourage local residents that work or go to school in Graham to ride a bicycle for commuting.
- PARKing Day – an opportunity annually for business owners and community members to temporarily transform on-street parking stalls into attractive, creative, and functional public spaces. This annual event allows for the public to imagine and build “parks” in a fun yet temporary way throughout downtown.

Residential Density Study – Marshall Street

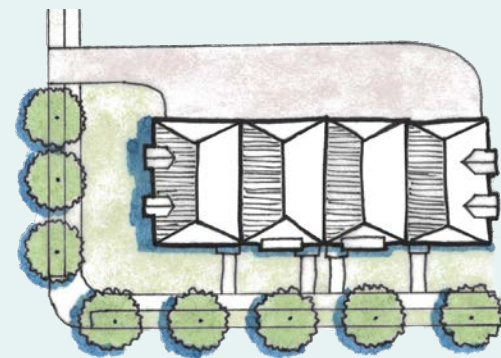
Residential density can be achieved through creative site planning and design. The existing site along Marshall Street is representative of the typical housing stock locate on the periphery of downtown Graham. Through a simple redesign of two parcels, several residential units can be accommodated and maintain the desired character of Graham.



Existing Site



Compact Single Family: 3 Units



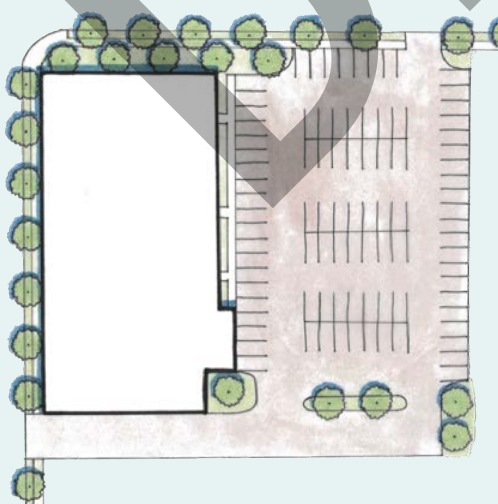
Townhouses: 4 Units

Gateway Corridor Development

Gateways Corridors are meant to welcome people as they enter a community and should appear visually different than surrounding areas. Buildings should address the street, all parking should be hidden from view unless on-street, inner-parcel connectivity and shared parking areas should be the norm, infill development should match the fabric of downtown, street trees should form an allee, and pedestrian facilities should be provided between the allee and building fronts. Through strong design, people are provided with a visual que of arrival to downtown, slow down, and places people first. To achieve the Gateway Corridor for all new development within the Main Street Corridor new development regulations and zoning ordinances must be adopted to fit Graham’s desired character.



Existing Site



Commercial

Data Collection

Transforming Downtown Graham will be incremental and collecting important data as a baseline and as projects are implemented will help to tell the story of how changes are impacting the community. Data collection should start in Phase 1 but is an on-going process that should continue through all recommended phases and beyond.

Project Phase: On-going

Data collection should include, but is not limited to the following:

- Routine bicycle and pedestrian counts throughout the downtown area
 - » Parking capacity and utilization counts for both vehicle and bicycle parking
- Residential activity in Downtown Graham
- New businesses and new industries in downtown
- Crash data for all transportation modes that includes but is not limited to:
 - » Type of crash
 - » Crash severity
 - » Modes of travel involved
 - » Location of crash (i.e., on-street vs. off-street, intersection vs. non-intersection)
 - » Street/intersection characteristics for on-street crashes
- Vacancies
- Annual evaluation of property values

Complete Sidewalks along Maple and Marshall (Elm to Gilbreath)

A completed sidewalk network approaching Downtown Graham will increase access from the surrounding neighborhoods and properties. Filling sidewalk gaps along Maple Street and Marshall Street are critical to the long-term success of Downtown Graham. NCDOT coordination on these sidewalks should be conducted concurrently with changes to Main Street, Court Square, and alternative truck routes.

Project Phase: Two

W. Market Street - Neighborhood Bikeway (W. Elm to Maple)

A neighborhood bikeway, sometimes referred to as a bicycle boulevard, along W. Market Street provides a low-stress connection for people on bicycles traveling from W. Elm Street or E. Webb Street toward Downtown Graham. Neighborhood bikeways should be designed to minimize vehicle operating speeds and reduce cut through traffic.

Project Phase: One

The W. Market Street neighborhood bikeway should consider the following:

- Intermittent curb extensions/bulb outs to narrow the travel way and slow turning speeds near intersections.
- Striping stop bars at the following stop-controlled intersections:
 - » North Street
 - » Washington Street
 - » Maple Street
- Formalizing parking areas where there is adequate width.
- Shared lane pavement markings and bicycle wayfinding signage.

W. Market Street - Slow Street (Maple to N. Main)

The continuation of the W. Market Street neighborhood bikeway is a one block slow street between Maple Street and N. Main Street. A slow street design removes barriers and allow for all users to move freely through and across the street. The slow street design and construction should be coordinated with the N. Main Street project to increase efficiency in construction.

Project Phase: Two - Three

Implementing the W. Market slow street should include:

- Consideration of a flush street based upon assessment of Court Square redesign.
- Options for decorative street materials such as brick or pavers.
- Wayfinding signage for all users.
- Street furnishings and lighting.
- Opportunities for public art.

Pine Street Bike Lanes

Bike lanes on Pine Street provide a clear east/west bikeway connection that currently does not exist in the City of Graham. Additionally, striping bike lanes reduces the existing outside lane width and could assist in reducing vehicular speeds. This project has already been in progress and should be implemented in the immediate-term.

Project Phase: One

As part of the S. Main Street project, a modified protected intersection should be considered to provide separation for people on bicycles as they approach S. Main Street along Pine Street.

Oneida Street - Neighborhood Bikeway (W. Market to W. Elm)

Much like W. Market Street neighborhood bikeway, this two-block section of Oneida Street should be designed to reduce cut through traffic and lower vehicle operating speeds. Linking W. Market Street to W. Elm Street via the Oneida Street neighborhood bikeway offers convenient access to Downtown Graham.

Project Phase: Two

The Oneida Street neighborhood bikeway should include:

- Curb extensions at Oneida Street and W. Market Street to slow turning vehicles and narrow travel lanes.
- Tighter curb radii at the intersection with W. Harden Street.
- Striped stop bars for Oneida Street at W. Harden Street.
- Shared lane pavement markings and bicycle wayfinding signage.
- Rectangular Rapid Flashing Beacon (RRFB) for crossing W. Harden Street.

N. Main Street Bike Lanes (Albright to Washington)

After the completion of the N. Main Street gateway roundabout at the intersection of Albright, bike lanes are recommended to connect residential areas to Downtown Graham by bicycle along the most direct route. Designating operation space for people riding bicycles will increase comfort along with narrowing travel lanes. The current width from curb to curb is approximately 32-feet. Reallocating space along this street would allow for up to 6-foot direction bike lanes along with 10-foot travel lanes.

Project Phase: Three

The N. Main Street bike lanes should consider:

- Conflict markings for all driveways and property curb cuts.
- Intersection treatments to prioritize bicycles and pedestrian at Washington Street
- Wayfinding and signage bicycles

McAden Street Bike Lanes

In conjunction with the implementation of the S. Main Street gateway roundabout and streetscape changes, bike lanes along McAden Street are recommended. Connecting Maple Street to Graham Middles School, bike lanes along McAden also intersect the proposed shared use paths along S. Main Street with direct access into the Court Square.

Project Phase: Two

Bike lanes on McAden Street should include:

- Clear wayfinding signage to local destinations including Court Square and Graham Middle School.
- Ramps from the bike lanes to separated shared use crossings of the roundabout at S. Main Street.
- Conflict markings for all driveways and property curb cuts.
- Considerations for arrival and dismissal of students at Graham Middle School by bicycle to mitigate conflicts with vehicles.

Branding/Wayfinding

The City of Graham should clearly define a brand for the downtown area along with wayfinding signage for all modes of transportation. The brand for the City of Graham should be incorporated into the streetscape improvements, wayfinding, and Court Square design. Branded wayfinding should be intuitive and direct users to a variety of destinations within Downtown Graham, including local attractions, institutional uses, and local parking. Signage should be applied consistently throughout Downtown Graham to ensure that residents and visitors can find their anticipated destination or discover something new in downtown.

Project Phase: One

STREETSCAPE + PUBLIC SPACE

Implementation Considerations

The Graham Downtown Master Plan provides a clear vision for the future of Graham. While conceptual designs have been developed, many details must be further explored before and during design and construction of each project. Additional attention must be given to the following as implementation progresses. These are outlined below.

NCDOT Coordination

With three major state routes converging in Downtown Graham; NC 87, NC 54, and NC 49, NCDOT's partnership is critical for successful implementation of this plan. NCDOT has been engaged throughout the life of the Graham Downtown Master Plan as alternative concepts were created, traffic studies discussed, and final recommendations revealed.

While the City and NCDOT have made great strides toward implementation and the betterment of Graham, many discussions and decisions still must be made. Critical discussions include:

- **Alternative Truck Route** – Currently semi-trucks are routed through Court Square, the heart of Graham. In order to create a vibrant destination within downtown, semi's need to be limited to delivery only not through traffic in the Square. An alternative truck route must be identified before Court Square can realize its full potential.
- **Road Diet** – Conceptually Main Street, North of Gilbreath to Pine Street will move to a three-lane cross section from a five-lane cross section. This will provide a gateway into downtown and multimodal facilities creating a complete street in Graham. NCDOT legislation states that a roadway over 18,000 vehicles per day is not an eligible candidate for a road diet. The current average annual daily traffic count is just over 19,000 vehicles per day. An updated traffic count and further traffic engineering must be performed. Alternatively, Graham established a tight grid network of streets with three north south connectors (Maple Street, Main Street, and Marshall Street). The combined capacity of three three-lane streets will hold approximately 54,900 vehicles per day. Utilizing its grid network, Graham has capacity to almost triple the number of vehicles it serves per day.
- **Road Swap** – The City and NCDOT have discussed the possibility of a road swap, Main Street from north of Gilbreath Avenue to Harden Street for Marshall Street. This swap has the potential to become an alternative truck route and provides the City with control of Main Street as it enters downtown.
- **NC 87 Designation** – Main Street's designation as NC 87 has both a positive and negative impact on downtown. The route brings commuters, visitors, and residents alike through the heart of downtown and potential revenue for downtown merchants. NC 87 is a major north south regional connector for the State of North Carolina that must be maintained; however, Main Street does not have to serve as its designation. An alternative designation has the potential to create an inviting and engaging downtown, free of commuters and semi-trucks. Currently, alternative designations are being looked at to include but not limited to; Maple Street to Harden Street, NC 54, and neighboring communities. Further exploration and discussion with NCDOT must take place.

Stormwater + Utilities

Stormwater conveyance and utilities are necessities of every community in the country yet are often overlooked. Graham has a number of utilities within the study area, including electrical, water, sewer, storm sewer, gas, telephone, and cable. Coordination with all franchise utilities will be necessary and should begin to take place in the immediate future.

Green infrastructure should be considered in every project as we think about creating resilient cities and landscapes. Daylighting traditionally hidden systems provides environmental, social, and economic benefits in addition to alleviating pressure on an aging or failing system. Stormwater conveyance, storage, and filtration must be given special attention during the design and construction of every project in Graham, not just downtown, for collective watershed management.

All stormwater and utility improvements should be made in concert with construction improvements in Graham to eliminate the need for later tearing up of the newly built street or public amenity and minimize taxpayer expense for

having to repeat a construction project. At the ribbon cutting of every project, Graham should have an inclusive system, street, utilities, and stormwater, with a minimum life span of 25 years before improvements are necessary.

Staffing + Maintenance

Graham is fortunate to have a public works department with the ability to perform minor repairs to major construction projects for the size of its community. The public works department, under its current leadership, has the knowledge and capability to construct each streetscape project presented in this plan, including Court Square. With proper funding and an increase in staff, Graham has the ability to utilize its public works department for construction and drastically reduce the cost of implementation.

Once constructed, Graham will need dedicated staff for routine maintenance of downtown. Routine maintenance may include landscaping, street sweeping, watering hanging baskets, refuse collection, banner hanging, and festival/market setup, breakdown, and clean up. At full build out of downtown a dedicated three-man crew with equipment must be established to keep Graham beautiful. This will require an increased fiscal year budget for public works but is a necessity to create a vibrant destination.

DRAFT

Court Square - Pilot Project

Court Square is the most underutilized public realm space within Graham. The concept plan presented at the recommendations reveal sparked excitement with the community of the potential the Square has. To continue the momentum a pilot restripe project has been identified as a Catalyst project. Through the implementation of a pilot restripe project the community will temporarily be able to experience the vision for Court Square while NCDOT coordination and funding take place to permanently construct Graham's post card. The concept plan will need to be turned into a temporary striping plan for rapid implementation. A pop-up gathering space will also need to be planned for a minimum of one-week installation. The City should hire a full-service engineering consultant firm with experience designing pilot projects for rapid implementation to walk the City through the process.

Project Phase: One

The pilot project should include the following:

- **Coordination:** The City must continue open communication with NCDOT and Alamance County and embrace its partnership as a symbiotic relationship.
- **Design:** The pilot project design will be a striping plan using temporary paint, landscape, and flexible delineators based on the conceptual plan for Court Square. The 15-foot circulatory route for motorists will clearly be defined and the new parking layout will be established. New splitter islands (pedestrian refuge islands) will be identified using striping and flexible delineators or planters.
 - » **Landscape:** Large street trees and planters should be brought in at the beginning of the pilot project to help the community realize the visions. The street trees will help define vehicle and pedestrian spaces, slow motorist to maintain safe motorist speeds, and define the public realm. The landscape should be present for a minimum of one-week.
 - » **Water Quality/Stormwater:** Court Square has four low spots, one in each corner. Currently each of the four corners have a triangular raised brick planter. During the pilot project, each corner should be reconstructed into bioretention stormwater facilities. Each corner will have a simple landscape palette and large street tree. This will be a permanent installation until the full reconstruction project begins.

- » **Pop up Park:** During the pilot project, the City should choose one corner and establish a pop-up park with café seating, lighting, and landscape for a minimum of one week.

- **Evaluation:** The City must collect data on travel behavior before the pilot project begins and while the project is still in place. Relevant data includes speed, percentage of drivers yielding to pedestrians, traffic volumes, number of heavy vehicles, crash rate, etc. The data gathered and evaluated will help provide substantial evidence for the need of the full reconstruction project.
- **Duration:** The community will need time to adjust to the new traffic pattern and design. For this reason, the pilot project must remain in place for a minimum of 30 days. Longer durations are encouraged for additional evaluation data to be collected.
- **Funding:** The City has received a better block grant and should use the funding to complete the pilot project and evaluation.
- **Planning Level Opinion of Probable Construction Cost:** See appendix

Court Square - Reconstruction

Riding on the momentum gained during the pilot restripe project for Court Square, Graham should take the next step and embark on the full reconstruction project. In order for Court Square to be realized, the City will need to formalize a maintenance agreement or road swap with NCDOT, relocate semi-truck through traffic to an adjacent route, and create a partnership with Alamance County. A full-service engineering consultant firm with experience designing shared streets should be hired to walk the City through the design and construction process. The design firm should have experience working within right-of-way, designing public realm spaces, and understand the design intent. Currently the concept plan is hand graphics over aerial. A conceptual plan in CAD over survey should be completed first to ensure all desired elements can be accomplished within the given right-of-way. Once complete, Graham's most valuable asset will once again become the heart of Graham and a regional destination for residents and visitors alike.

Project Phase: Two

The reconstruction project should include the following:

- **Coordination/Evaluation:** The City must continue open communication with NCDOT and embrace its partnership as a symbiotic relationship. Results from the pilot restripe project must be evaluated to inform decision making and the design process. Graham and Alamance County partnership must be solidified. The City should proactively engage merchants and property owners throughout the life cycle of design to prepare them for construction. Success stories from the Elm Street Construction project should be shared.
- **Funding:** At the completion of the restripe project, the City should identify funding sources and revenue streams for full reconstruction. If pursuing a competitive grant, analysis and results from the restripe project will provide Graham a leg up on the competition as grantors look highly on pilot projects being implemented prior to application.
- **Design:** Court Square will become a flush street, void of curbs, making it inclusive for all ages and abilities and reestablish the heart of Graham. Brick pavers will make up the 15-foot circulatory route for motorist and on-street parking as well as utilized in the pedestrian crosswalks. Each of the four corners that currently serve cars will be reallocated to the public realm. The northwest corner could include a permanent market space and update park design of Sesquicentennial Park, the northeast corner could have gather space with active or passive amenities, the southwest corner will become an outdoor dining venue with catenary lighting, and the south east corner will have an open lawn for passive recreation. The Historic Courthouse grounds will receive an updated landscape and drop-off plaza to the north and south.
 - » **Material Palette:**
 - **Landscape:** A lush landscape palette of legacy street trees and sweeping masses of flowering shrubs, ornamental grasses, perennials, and groundcover should fill Court Square. A landscape palette has been established for Graham in the appendix.
 - **Hardscape:** Three primary materials should be used in Court Square for all hardscape surfaces; brick, granite, and concrete. A hardscape material palette has been established for Graham in the proceeding appendix.
 - » **Water Quality/Stormwater:** A new above and

below ground drainage system will be constructed to include bioretention cells, underground storage tanks, and traditional stormwater controls. Permeable brick pavers may be used in the parking spaces around the Square and in the Courthouse drop-off plazas to aid in the infiltration of stormwater.

- » **Utility:** A full reconstruct of all utilities in Court Square including undergrounding all overhead lines should be constructed. By upgrading all utilities during construction, Graham will be ready for an influx of new businesses and better serve its current merchants.

Elm Street

Elm Street, both west and east 100 blocks, will serve as Graham's primary festival street. The project will focus on public realm improvements along the street and at both Maple and Marshall Street intersections. The City currently owns and maintains Elm Street and already utilizes the street for festivals. A full-service engineering consultant firm with experience designing festival streets should be hired to walk the City through the design and construction process. The design firm should have experience working within right-of-way, designing public realm spaces, and understand the design intent. Currently the concept plan is hand graphics over aerial. A conceptual plan in CAD over survey should be completed first to ensure all desired elements can be accomplished within the given right-of-way. Once complete residence and visitors will be greeted with a vibrant brick street overflowing with life along the sidewalks shaded by legacy street trees.

Project Phase: One

The Elm Street project should include:

- **Coordination:** Graham owns and maintains Elm Street and does not need prior approval for construction. The City must proactively engage merchants and property owners throughout the life cycle of design to prepare them for construction.
- **Funding:** Capital improvement money is currently allocated for utility improvements for both West and East Elm Street. The streetscape project should happen in concert with the utility improvements as the street will be torn up during this time. Graham should allocate general funds, seek a bond, or look for grants to complete the project.

- **Design:** Elm street will become a brick street with 11-foot travel lanes and parallel parking. This reallocation of space will allow for 15-foot sidewalks and a 13-foot furnishing/landscape zone resulting in an unparalleled and flexible public realm space.
 - » **Material Palette:**
 - **Landscape:** A lush landscape palette of legacy street trees and sweeping masses of flowering shrubs, ornamental grasses, perennials, and groundcover should fill Elm Street. A landscape palette has been established for Graham in the appendix.
 - **Hardscape:** Three primary materials should be used on Elm Street for all hardscape surfaces; brick, granite, and concrete. A hardscape material palette has been established for Graham in the proceeding appendix.
 - » **Water Quality/Stormwater:** A new above and below ground drainage system will be constructed to include bioretention cells, tree bump outs, and traditional stormwater controls. Valley gutters will be installed between parallel parking spaces and the street to further the distance of water confluence from storefronts. The Elm Street trunk line will be reconstructed and the Maple Street trunk line will be evaluated for need of replacement.
 - » **Utility:** A full reconstruct of all utilities on the 100 blocks of Elm Street including undergrounding all overhead lines should be constructed. By upgrading all utilities during the streetscape project, Graham will be ready for an influx of new businesses and better serve its current merchants and ensures the street will not be torn up to make repairs in the near future.
- **Planning Level Opinion of Probable Construction Cost:** See appendix

Main Street - South

South Main Street is the southern gateway into Graham from Interstate 40/85 to Court Square. Currently this uninviting corridor features a wide five-lane roadway, high speeds, and a lack luster landscape. A new single lane roundabout at McAden Street will serve as a gateway to Graham as well as aid in slowing traffic speeds. A road diet to three-lanes will provide for a 12-foot shared use path on each side of the road and a lush tree lined street that funnels attention to Court Square. For this to become a reality additional traffic analysis will need to be

performed with up to date traffic counts and presented to NCDOT. The City should hire a full-service engineering consultant firm with experience transforming corridors to walk the City through the design and construction process of South Main Street. The design firm should have experience working within right-of-way, designing multimodal facilities, and understand the design intent. While the concept plan was designed using a new survey additional items need to be explored such as center landscape medians, driveway consolidation, and inner-parcel connectivity to ensure.

Project Phase: Two

The Main Street project should include:

- **Coordination:** At this stage, negotiations for a road diet and or ownership for Main Street should be complete. The City and NCDOT should have successfully negotiated the desired outcomes for Main Street. The City and NCDOT partnership should continue. The City should proactively engage merchants and property owners along the corridor throughout the life cycle of design to prepare them for construction. Success stories from the Elm Street Construction project should be shared.
- **Funding:** During negotiations of Main Street with NCDOT, Graham should seek construction capital from NCDOT to make all improvements and couple Interstate on- and off- ramp improvements using Spot 6.0 money. Graham should also apply for federal grant money from the MPO and allocate general funds.
- **Design:** South Main Street will become the southern gateway for Graham beginning with a single lane roundabout at McAden Street, a road diet transforms the street into 11-foot travel lanes, 12-foot turn lane, a 12-foot landscape buffer, and 12' shared use path. At Pine Street, the street narrows to 11-foot travel lanes with angled parking. This reallocation of space will allow for 12-foot sidewalks and an 8-foot furnishing/landscape zone resulting in an unparalleled and flexible public realm space.
 - » **Material Palette:**
 - **Landscape:** A lush landscape palette of legacy street trees and sweeping masses of flowering shrubs, ornamental grasses, perennials, and groundcover should fill Main Street. A landscape palette has been established for Graham in the proceeding appendix.

- **Hardscape:** Four primary materials should be used on Main Street for all hardscape surfaces; asphalt, brick, granite, and concrete. A hardscape material palette has been established for Graham in the proceeding appendix.
- » **Water Quality/Stormwater:** Prior to construction the Main Street trunk line should be evaluated for need of replacement and conveyance capacity from upstream drainage. Tree island bump outs should be installed in areas with angled parking, bioretention facilities should be constructed along the shared use path, and new catch basins installed throughout the corridor. Valley gutters should be installed between parallel parking spaces and the street to further the distance of water confluence from storefronts.
- » **Utility:** A full reconstruct of all utilities along the corridor should be constructed including undergrounding all overhead utility lines on the 100 blocks. By upgrading all utilities during the streetscape project, Graham will be ready for an influx of new businesses and better serve its current merchants and ensures the street will not be torn up to make repairs in the near future.

Main Street. The City and NCDOT partnership should continue. A recommended outcome or coordination would be that Graham would begin to own and maintain North Main Street from Harden Street to Albright Avenue and would not need prior approval for construction. The City should proactively engage merchants and property owners along the corridor throughout the life cycle of design to prepare them for construction. Success stories from the South Main Street Construction project should be shared.

- **Funding:** During negotiations of Main Street with NCDOT, Graham should seek construction capital from NCDOT to make all improvements. Graham should also apply for federal grant money from the MPO and allocate general funds.
- **Design:** North Main Street will become the northern gateway for Graham beginning with a single lane roundabout at Albright Avenue, a reallocation of street space will provide 11-foot travel lanes, angled parking, an 8-foot landscape buffer, and 12' sidewalk. This reallocation of space will complete the unparalleled and flexible public realm space along Main Street.

» **Material Palette:**

- **Landscape:** A lush landscape palette of legacy street trees and sweeping masses of flowering shrubs, ornamental grasses, perennials, and groundcover should fill Main Street. A landscape palette has been established for Graham in the proceeding appendix.
- **Hardscape:** Four primary materials should be used on Main Street for all hardscape surfaces; brick, granite, concrete, and asphalt. A hardscape material palette has been established for Graham in the proceeding appendix.

- » **Water Quality/Stormwater:** Prior to construction the Main Street trunk line will be evaluated for need of replacement and conveyance capacity of upstream needs. Tree island bump outs will be installed in areas with angled parking. Valley gutters will be installed between parallel parking spaces and the street to further the distance of water confluence from storefronts.
- » **Utility:** A full reconstruct of all utilities along the corridor will be constructed including undergrounding all overhead utility lines on the 100 blocks. By upgrading all utilities during the streetscape project, Graham will be ready for an influx of new businesses and better serve its current merchants and ensures the street will not be torn up to make repairs in the near future.

Main Street - North

North Main Street is the northern gateway into Graham from Albright Avenue to Court Square. Currently this corridor features an extremely wide two-lane roadway with on-street parking, high speeds, and a lack luster landscape. A new single lane roundabout at Albright Street will serve as a gateway to Graham as well as aid in slowing traffic speeds and fixing geometry issues at the intersection. Excess street width will provide additional sidewalk width and an 8-foot furnishing/landscape zone. The City should hire a full-service engineering consultant firm with experience transforming corridors to walk the City through the design and construction process of North Main Street. The firm should have experience working within right-of-way to reallocate space in the public realm to accommodate all modes and understand the design intent. While a concept was designed using a new survey additional items need to be explored such as driveway consolidation and inter-parcel connectivity.

Project Phase: Three

The Main Street project should include:

- **Coordination:** The City and NCDOT should have successfully negotiated the desired outcomes for

Funding

Funding is a critical element for successful implementation of any project. While Graham is a financially healthy community, it is not financially able to implement the Downtown Master Plan in its entirety on its own. Key partnerships and resources will need to be made, identified, and applied for to leverage existing dollars for more construction funds. Outlined below are key partnerships and potential funding opportunities for Graham to take advantage of, many of which already are.

Partnerships

Alamance Arts

Graham is home to Alamance Arts whose mission is to shape the cultural identity of Alamance County by making art a tangible presence in the lives of its citizens. We strive to enhance the quality of life by engaging people in a diverse array of art through the delivery of programming and education and through the provision of facilities, advocacy, promotion and funding. Local and world-renowned artists (painters, musicians, sculptors, etc.) are showcased throughout Graham on a daily basis. Graham can work with Alamance Arts to commission custom art pieces in the gateway roundabouts, within Court Square, and murals on surrounding business downtown.

New Leaf Society

A partnership with the New Leaf Society can help create a greener Graham. The New Leaf Society was formed in 2007 as a private non-profit organization, designed to work through partnerships to enhance the quality of life and economic prosperity in Alamance County by planting trees and creating landscaping projects. Modeled after the Noble Tree Society in Spartanburg, SC, New Leaf Society's purpose is to generate private funds to support city/community beautification programs. Through in-kind landscape installations by the Society, Graham's gateway corridors can become an iconic allee of legacy street trees.

Alamance-Burlington School System

The Graham Downtown Master Plan is a legacy building project for the next generation. To fully realize this plan, it may take ten to fifteen years of diligent work. Students, especially those in Graham City Schools, must be engaged throughout the life of the project. Graham has a

desire for graduates to come back and become employed in the City. Graham must give graduates a reason to come back. Students should be provided the opportunity to participate in additional visioning workshops, during design reviews, and help implement the pilot restripe of Court Square. By allowing students voice to be heard, Graham will bring up the next generation with civic pride for their community.

Possible Funding Resources

Below are some of the possible funding sources that may help Graham move the plan to reality:

Transportations Funds

Better Utilizing Investments to Leverage Development (BUILD) Transportation Discretionary Grant:

The program provides a unique opportunity for the DOT to invest in road, rail, transit and port projects that promise to achieve national objectives. Previously known as Transportation Investment Generating Economic Recovery, or TIGER Discretionary Grants, Congress appropriated \$900 million for FY 2019 to be awarded by the Department of Transportation for national infrastructure investments through September 30, 2021. Like the FY 2017 TIGER program, the FY 2019 BUILD program will also give special consideration to projects which emphasize improved access to reliable, safe, and affordable transportation for both rural and urban communities, such as projects that improve infrastructure condition, address public health and safety, provide innovative multi-modal facilities, and promote regional connectivity or facilitate economic growth. If awarded, this highly competitive grant can provide a maximum of \$25 million toward infrastructure. Communities can apply in successive years for additional phases of a project. Applicants must detail the benefits their project would deliver for five outcomes: safety, state of good repair, economic competitiveness, environmental sustainability, and quality of life.

Strategic Transportation Prioritization (SPOT):

The Strategic Transportation Prioritization Process is the methodology that NCDOT uses to develop the State Transportation Improvement Program (STIP). The process involves scoring all roadway, public transportation, bicycle, pedestrian, rail, and aviation projects on a number of criteria. Metropolitan Planning Organizations (MPOs), Rural Planning Organizations (RPOs), and the NCDOT

Division offices also contribute to the final project score by assigning local priority points to projects. The deadline to submit a project to the Burlington-Graham MPO for review is December 20, 2019.

NCDOT Statewide Contingency Fund:

The Statewide Contingency Fund is a \$12 million fund administered by the Secretary of Transportation. Funding was established for statewide rural or small urban highway improvements related to transportation enhancements to public roads/public facilities and spot safety projects. Request should be submitted to the Division Engineer providing technical information such as location, improvements being requested, timing, justification, etc. for thorough review. If approved, the Division Engineer will present to the Project Review Committee which makes recommendations for funding to the Secretary.

NCDOT High Impact/Low Cost Funds:

Established in 2017 to provide funds to complete low cost projects with high impacts to the transportation system including intersection improvement projects, minor widening projects, and operational improvement projects. Funds are allocated equally to each of the 14 NCDOT Divisions. Each division is allowed to create their own scoring formula; however, at a minimum it must consider the average daily traffic and proposed additional traffic, safety issues, condition of existing roadway, and distance and radius of intersecting roadways. Projects have a maximum cost of \$1.5 million unless otherwise approved from the Secretary of Transportation. Request should be submitted to the Division Engineer providing technical information such as location, improvements being requested, timing, justification, opinion of probable cost, etc. for thorough review. If approved, the Division Engineer will present to the Chief Engineer for review and presentation to the Project Review Committee which makes recommendations for funding to the Secretary.

NCDOT Economic Development Funds:

Established to expedite transportation projects that promote commercial growth as well as either job creation or job retention. Funding is allocated based on the amount of new & retained jobs that will be created by the construction of a facility. Each job provides \$2,500 with a maximum of \$400,000 per project unless otherwise approved by the Secretary of Transportation. Request should be submitted to the Division Engineer providing technical information such as location, improvements being requested, timing, justification, etc. for thorough review. If approved, the Division Engineer will present to the Project Review Committee which makes

recommendations for funding to the Secretary.

NCDOT Small Construction Funds:

Established in 1985, each NCDOT Division administers \$2 million of funds for small-scale improvement projects in urban areas. Projects have a maximum cost of \$250,000 unless otherwise approved from the Secretary of Transportation. Requests for small urban funds may be made by municipalities, counties, businesses, schools, and industrial entities. Request should be submitted to the Division Engineer providing technical information such as location, improvements being requested, timing, justification, etc. for thorough review.

NCDOT Spot Improvement Program:

The Division of Bicycle and Pedestrian Transportation (DBPT) budgets \$500,000 per year for "spot" safety improvements throughout North Carolina. Eligible improvements include drain grate replacement, bicycle loop detectors, pedestrian signals and other small-scale improvements. These funds are used for small-scale projects not substantial enough to be included in the STIP. Proposals should be submitted directly to the Division of Bicycle and Pedestrian Transportation.

Main Street Solutions Fund:

The Main Street Solutions Fund supports small businesses in designated micropolitans located in Tier 2 and Tier 3 counties or designated North Carolina Main Street communities. The grants assist planning agencies and small businesses with efforts to revitalize downtowns by creating jobs, funding infrastructure improvements, and rehabilitating buildings.

Stormwater + Utility Funds

USDA Community Facilities Direct Loan & Grant Program:

This program provides affordable funding to develop essential community facilities in rural areas. An essential community facility is defined as a facility that provides an essential service to the local community for the orderly development of the community in a primarily rural area, and does not include private, commercial or business undertakings. Communities with a population of 20,000 people or less are encouraged to apply. The loan/grant can be used for public facilities such as town halls, courthouses, airport hangars, or street improvements (especially those with green infrastructure) and public safety services such as fire departments, police stations, prisons, police vehicles, fire trucks, public works vehicles or equipment. Contact a local USDA department for more details.

Clean Water State Revolving Fund (CWSRF):

Established in 1987, these funds are administered by the N.C. Department of Environment Quality Division of Water Infrastructure. The program provides up to \$30 million in funding for wastewater treatment, wastewater collection, reclaimed water, stormwater BMPs, stream restoration, and energy efficiency upgrades at treatment or collection systems. Local governments are encouraged to apply for low-interest loans, generally half of market interest rates. In some cases, 0% interest loads are available for Green Projects. Construction must start within 24 months of letter of intent to fund.

Community Development Block Grant (CDBG) Infrastructure:

Established in 2013, these funds are administered by the N.C. Department of Environment Quality Division of Water Infrastructure. The program provides up to \$2 million in funding over a 3-year period to provide/upgrade drinking water and wastewater infrastructure project that meet the HUD Low and Moderate Income (LMI) threshold.

Building Funds

CDBG Economic Development Building Reuse Grants:

These funds are administered for renovation and up-fitting of vacant industrial and commercial buildings for economic development purposes. The building reuse program is designed to return vacant industrial/commercial buildings to economic use for new and/or expanding business and industry. The ultimate goal of the Building Reuse program is to provide jobs for low and moderate-income persons (LMI). Each job provides \$12,500 with a maximum of \$500,000 per award depending on company statistics. A local government applicant must propose a project in conjunction with a private for-profit business that proposes to restore a vacant building to economic use resulting in the creation

of permanent, full-time jobs by the project company. A job is considered full-time if the employee works at least 1600 hours per year. To be eligible, documentation must be provided showing the building has been vacant thirty (30) consecutive days or more prior to the date of the pre-application conference. CDBG funds for this category are limited to a maximum of \$750,000 per unit of government. The grant amount is calculated based on \$20,000 per job for eligible businesses and \$12,000 per job for all other businesses.

North Carolina State Building Reuse Grant:

Established under the North Carolina Department of Commerce, this program provides grants to local governments for the renovation of vacant buildings within their community to attract new businesses. Grants are available to support the renovation of buildings that have been vacant for a minimum of three months prior to application deadlines. Each job provides \$12,500 with a maximum of \$500,000 per award depending on company statistics.

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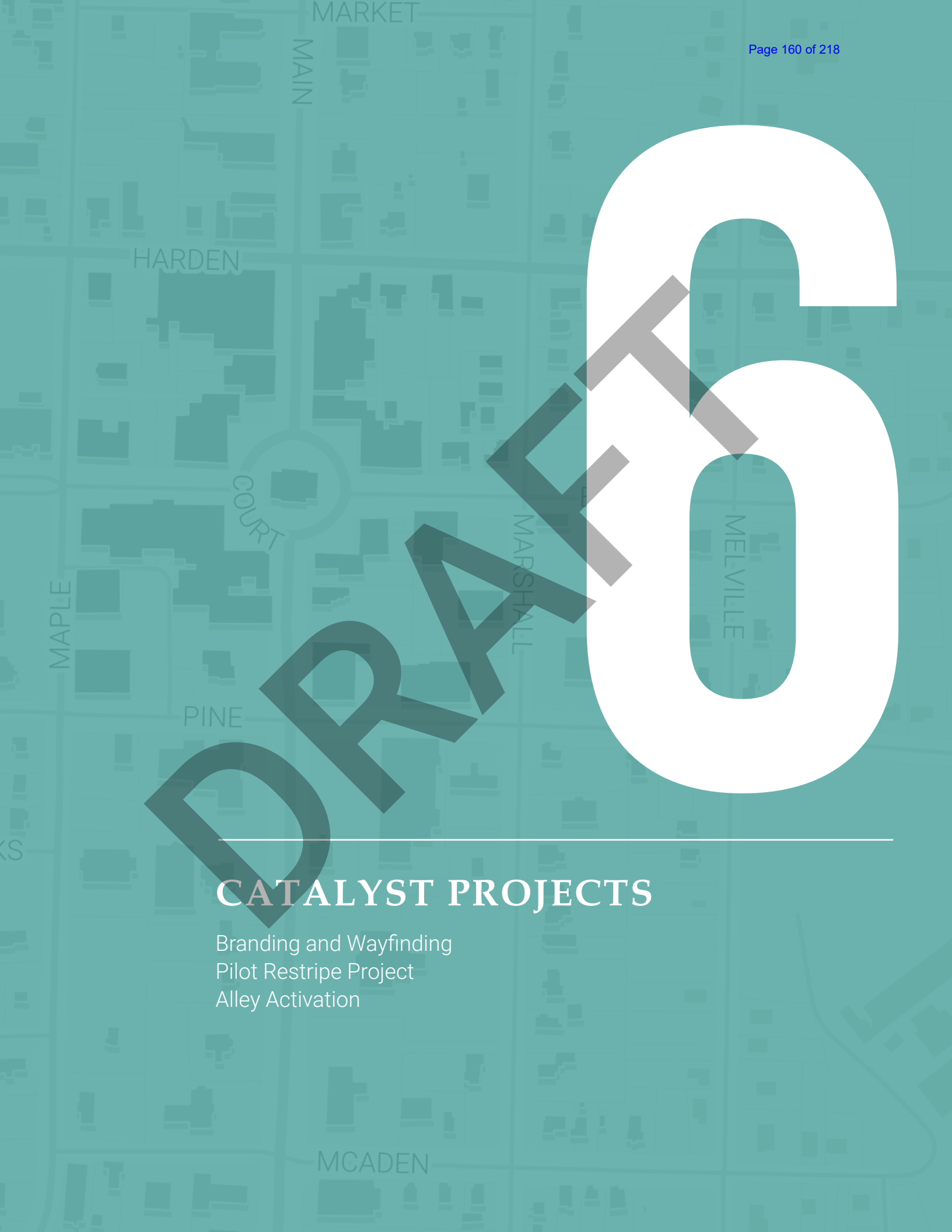
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CATALYST PROJECTS

Branding and Wayfinding
Pilot Restripe Project
Alley Activation



CATALYST PROJECTS

The City of Graham Downtown Master Plan is an important step in revitalizing Downtown Graham. This document creates a vision for the future, and it outlines a roadmap of initiatives and projects that will guide the city to a better future.

There is not a single project that will transform downtown. To fulfill the vision laid out in this plan, a collection of projects, programs, and community support will move Graham closer to maximizing its potential. The City of Graham can, however, kickstart development by implementing “catalyst” projects, which are projects that will quickly make a significant impact with little investment. Catalyst projects outlined in this section have the greatest potential to enliven downtown while being cognizant of available funding. They will generate energy and enthusiasm in the community, continue the movement gained from the development of the plan, and will stimulate the market to ripen for future development.

In most cases, catalysts projects are not final stages of a design or program and should be implemented concurrently or before the first phase of recommendations. In general catalyst projects are just the beginning of larger-scale capital improvement projects. In some cases, the catalyst project may not achieve all goals of the final project but rather offer a “taste” of what the final project will become and show the community that the City is dedicated to creating a vibrant atmosphere downtown. This should be the intent when implementing catalyst projects.

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Branding And Wayfinding

Graham currently has a recognizable brand, which pays homage to Court Square. A unique brand is a key component to vibrant downtowns. Graham should continue to build out this brand; it can tell a story for the core district of the community. The brand should become a symbol for downtown and its vibrant future.

A simple way to communicate this branding is through a comprehensive system of wayfinding signage in downtown. Wayfinding directs residents and visitors to key destinations within a community. Wayfinding signage in Graham must focus on downtown and Court Square. The City has already begun implementing a well-designed

wayfinding system for parking and should expand this to accommodate destination wayfinding throughout town.

Wayfinding can come in many forms, such as small placards along a sidewalk or street to branded banners. Both the simple and elaborate wayfinding signage may be appropriate to clearly identify the downtown district, the public spaces, and retail and dining districts. See examples of successful wayfinding below.

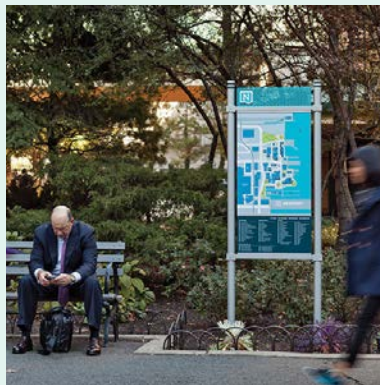
ANN ARBOR, MICHIGAN



Pedestrian, vehicular, and parking wayfinding in downtown Ann Arbor

www.annarbor.com, www.corbindesign.com

NEWPORT, NEW JERSEY



Wayfinding in Newport includes parking, pedestrian routing, and vehicular wayfinding

www.twotwelve.com

Pilot Restripe Project

“Pilot” restriping projects are temporary, small-scale project intended to simulate proposed roadway changes using paint, planters, and street furniture. This plan proposes a pilot restriping in the Court Square.



Restriping project in Athens, GA

Pilot restriping projects serve several purposes. First, they allow people to practice driving, walking, bicycling, and wheeling in a new setting. Sometimes, changes in road configurations can be confusing and cause people to feel uncomfortable or unsafe. In Court Square, the proposed design will move traffic in a slightly different pattern. Pilot

striping allows for users to gain confidence before the infrastructure changes are put in place.

Pilot projects are a great way to more clearly understand the changes in traffic patterns and user behavior in downtown before any permanent infrastructure is built. City staff should collect data before and during the pilot project for comparison of many factors, including speed limits, crashes, “near misses,” yield behaviors, and number of heavy vehicles. Collecting and analyzing this data will help city staff and the design team tweak the design as needed before construction drawings are finalized.

Finally, a pilot restriping project allows the community to get involved in creating their downtown space. With clear programming and direction, community members can help city staff transform Court Square for the temporary restriping. This is an opportunity to engage students at Graham Middle School and Graham High School, non-profits and civic organizations, and local businesses. Including volunteers in a pilot project’s set up fosters excitement for more change. It can be difficult to imagine what community change will look like, but the pilot project will allow people to experience what the final project will bring to downtown.

Guidelines:

- **The City of Graham should make the Court Square restriping pilot process a community event. Community volunteers helping with the restriping process should be given clear, direct tasks. Coordinate with local artists, vendors, musicians, or other community organizations to create a fun atmosphere.**
- **The pilot project should be left in place a minimum of 30 days (if not several months). This provides the community a chance to acclamate to the change and their travel behavior before the project is removed.**
- **The City must collect data on travel behavior before the restriping process begins and while the pilot project is still in place. Relevant data include speed limit, percentage of drivers yielding to pedestrians, traffic volumes, number of heavy vehicles, number of people bicycling and walking, and crash rates.**

Alleyway Activation

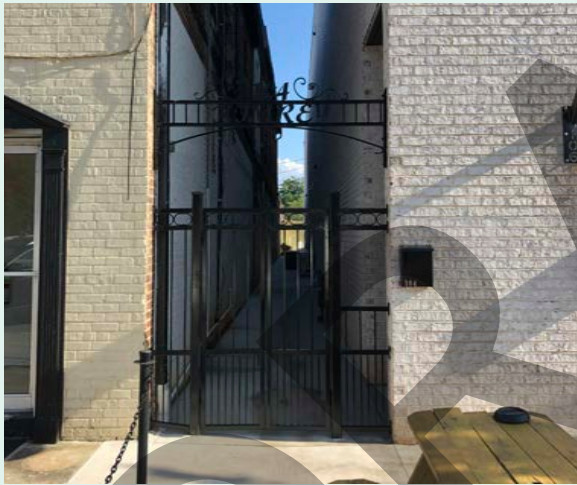
Alleyways between buildings offer opportunities to extend the pedestrian realm from the streetscape to public parking and rear entryways into businesses. Typically, alleyways are neglected, poorly lit, and often forgotten spaces. By installing catenary lighting, murals and artwork on the open building canvas, and planter boxes to demarcate entrances, alleyways can become integral to the fabric of downtown. As noted in Chapter Three, the existing alleys in Graham can connect Court Square and the 100 Blocks to parking lots and alleviate the perceived pressure for on-street parking.

An activated public alleyway is an ideal catalyst project; it is a small-scale and low-cost investment that is also highly visible and fun. Community members can be

invited to participate in beautification through community artwork, donations of plant materials and small street furniture, hang lights, or clean up. Alternatively, local artists can create murals along alleyway walls.

The alleyway beside HiFi Records is a prime candidate for the catalyst project. Not only does this alleyway connect a parking lot to Court Square, but there is also existing artwork on one of the alleyway walls. An expanded mural, plants, and lighting are all that is needed to create an inviting space. The same process could be applied to the alleyway between Green & McClure and Suttons, and next to Beer Co Graham (if an alleyway is privately owned the City should work toward a partnership with the owner to activate the alleyway).

EXISTING ALLEYWAYS OF GRAHAM



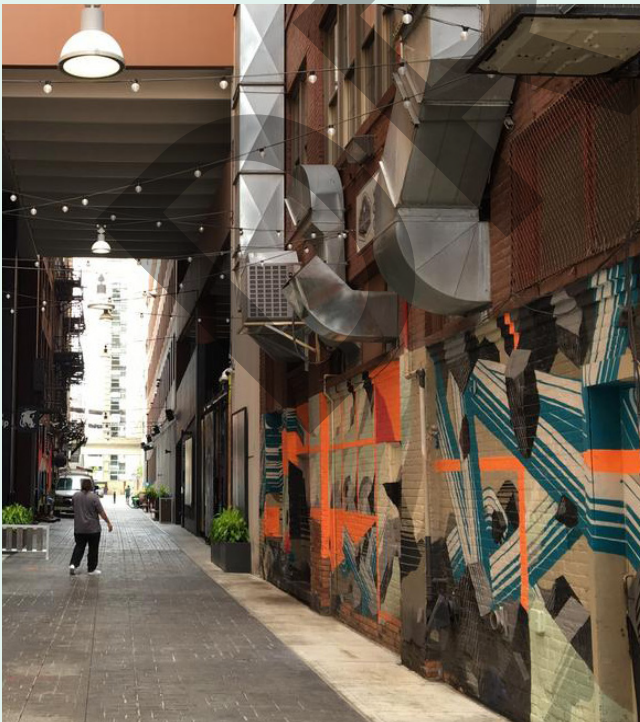
ALLEYWAY EXAMPLES



www.aspiremetro.com



www.pinterest.com



www.bizjournals.com



www.depostalesurbanas.com

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APPENDIX

A

AMENITY TOOLKIT

Landscape Palette

Hardscape Palette



AMENITY TOOLKIT

The City of Graham realizes the visual quality of downtown is paramount to its success. This goes beyond storefronts and building facades and focuses on streetscape elements such as the roadway, sidewalks, crosswalks, furnishings, lighting, plant material, parking, intersection designs, material palette, and public art installations. The combination of these elements, when carefully designed and woven into the existing landscape fabric of Graham, create a vibrant and inviting atmosphere that is engaging, meaningful, and encourages economic growth. The following pages provide guidelines for the development of Downtown Graham's streetscape, acknowledging its past and planning for the future.

Amenity Toolkit Components

Landscape Palette:

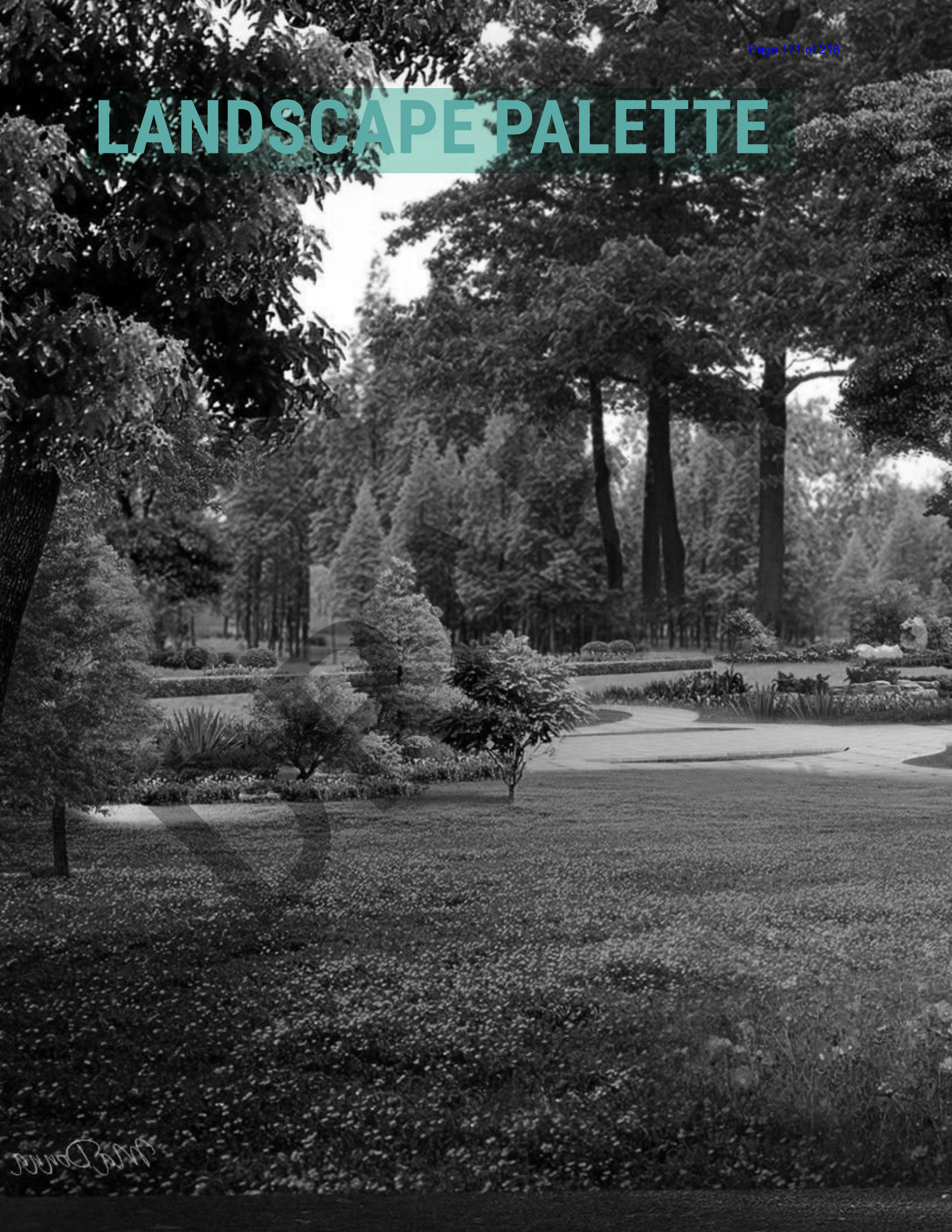
The landscape palette must be visually appealing, utilizing mass drifts of perennial flowers, shrubbery, and create an urban canopy of stately trees. The species of plant material have been selected based on their ease of maintenance, indigenous status, and climate of growth. The landscape must draw visitors and residents to downtown and leave a lasting legacy for years to come.

Hardscape Palette:

The hardscape palette must be bold yet understated and fit the existing historic nature of Graham. Furnishings, light fixtures, and surface materials all work together to establish the destination. A nod toward traditional furnishings with a modern take has been chosen to set the stage for Graham.

The following pages list and describe hardscape materials, plant species, characteristics, and seasonal interests. All plant material follows the North Carolina Department of Transportation (NCDOT) specifications for roadside plantings and height restrictions.

LANDSCAPE PALETTE



Wendy Brown



CANOPY TREES



COMMON NAME: Aeryn Trident Maple

BOTANICAL NAME: *Acer buegerianum* 'Aeryn'

CHARACTERISTICS: Tolerates poor soils, moderate drought conditions, and heavy clay soil.

HEIGHT: 25' - 35'

SEASONAL INTEREST: Lustrous dark green foliage, burgundy fall color.



COMMON NAME: Black Gum

BOTANICAL NAME: *Nyssa sylvatica*

CHARACTERISTICS: Slow-growing, native deciduous tree with a stright trunk and rounded crown.

HEIGHT: 30' - 50'

SEASONAL INTEREST: Scarlet fall color.



COMMON NAME: Chinese Pistache

BOTANICAL NAME: *Pistacia chinensis*

CHARACTERISTICS: Small deceduous tree with oval rounded crown.

HEIGHT: 30' - 35'

SEASONAL INTEREST: Fall color varies between yellow, orange, and red.



COMMON NAME: Swamp White Oak

BOTANICAL NAME: *Quercus bicolor*

CHARACTERISTICS: Medium native deciduous tree with a broad rounded crown tolerant of poor soils.

HEIGHT: 50' - 60'

SEASONAL INTEREST: Fall color is generally yellow but can be red.



COMMON NAME: Willow Oak
BOTANICAL NAME: *Quercus phellos*
CHARACTERISTICS: Medium to large native deciduous tree with willow-like leaves and relatively fast growth rate.
HEIGHT: 40' - 60'
SEASONAL INTEREST: Dark, irregularly-furrowed trunks develop on mature trees.



COMMON NAME: Shawnee Brave Bald Cypress
BOTANICAL NAME: *Taxodium disticum* 'Shawnee Brave'
CHARACTERISTICS: Medium native deciduous tree tolerant of poor soils and has a narrow columnar form.
HEIGHT: 75'
SEASONAL INTEREST: A stately tree with attractive bark and iconic bronze fall color.



COMMON NAME: American Linden
BOTANICAL NAME: *Tilia Americana*
CHARACTERISTICS: Medium to large deciduous tree with an ovate-rounded crown.
HEIGHT: 50' - 80'
SEASONAL INTEREST: Fragrant pale yellow flowers in late spring.



COMMON NAME: Patriot Elm
BOTANICAL NAME: *Ulmus* x 'Patriot'
CHARACTERISTICS: Vase or fountain shaped arching branch structure. Resistant to Dutch Elm disease and tolerant of urban conditions.
HEIGHT: 40' - 50'
SEASONAL INTEREST: Glossy green foliage, yellow fall color.



COMMON NAME: Green Vase Japanese Zelkova
BOTANICAL NAME: *Zelkova serrata* 'Green Vase'
CHARACTERISTICS: Medium to large deciduous tree with an upright, vase-shaped crown.
HEIGHT: 50' - 80'
SEASONAL INTEREST: Fall color varies from yellow, yellow-orange, to red-brown.

ACCENT TREES



COMMON NAME: Oklahoma Redbud
BOTANICAL NAME: *Cercis reiformis* 'Oklahoma'
CHARACTERISTICS: Native tree, round to vase shape canopy, drought tolerant.
HEIGHT: 20' - 25'
SEASONAL INTEREST: Showy pink flowers in early spring, dark green glossy leaves, yellow fall color.



COMMON NAME: Flowering Dogwood
BOTANICAL NAME: *Cornus florida*
CHARACTERISTICS: Small native deciduous tree with a broadly-pyramidal shape
HEIGHT: 15' - 30'
SEASONAL INTEREST: White flowers (bracts) in early spring and red fall color.



COMMON NAME: Robin Holly
BOTANICAL NAME: *Ilex x 'Conin'*
CHARACTERISTICS: Small evergreen tree with a pyramidal shape
HEIGHT: 15' - 20'
SEASONAL INTEREST: Dark green foliage year-round with bright red berries.



COMMON NAME: Teddy Bear Magnolia
BOTANICAL NAME: *Magnolia grandiflora* 'Southern Charm'
CHARACTERISTICS: Naive evergreen tree with tight branching structure.
HEIGHT: 30' - 50'
SEASONAL INTEREST: Lovely white fragrant 6" to 8" wide flowers May thru November.



COMMON NAME: Saucer Magnolia
BOTANICAL NAME: *Magnolia x soulangeana*
CHARACTERISTICS: Small deciduous tree with a rounded crown.
HEIGHT: 20' - 25'
SEASONAL INTEREST: Showy pink flowers with white interiors in spring.



COMMON NAME: Autumnalis Cherry
BOTANICAL NAME: *Prunus subhirtella 'Autumnalis'*
CHARACTERISTICS: Medium deciduous tree with a symmetrical crown.
HEIGHT: 40' - 50'
SEASONAL INTEREST: Heavy bloom of pink flowers in spring.

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SHRUBS



COMMON NAME: Wintergreen Boxwood
BOTANICAL NAME: *Buxus microphylla* 'Wintergreen'
CHARACTERISTICS: Medium sized hedge and foundation plant to be kept at 24"-36" tall and wide. Very tolerant of pruning or shearing and grows in full sun to shade.
Size: 48"H x 48"W
SEASONAL INTEREST: Winter hardy lush green foliage.



COMMON NAME: Vintage Jade Distylium
BOTANICAL NAME: *Distylium* 'Vintage Jade' PP 23,128
CHARACTERISTICS: Low maintenance mounding evergreen tolerant of poor urban soils.
SIZE: 36"H x 48"W
SEASONAL INTEREST: Dark green evergreen leaves with small red flowers in winter.



COMMON NAME: Bobo Hydrangea
BOTANICAL NAME: *Hydrangea paniculata* 'ILVOBO' PP 22,782
CHARACTERISTICS: Low maintenance compact mounding shrub tolerant of urban soils and full sun.
SIZE: 36"H x 48"W
SEASONAL INTEREST: Engulfed by large white flowers in summer.



COMMON NAME: Soft Touch Holly
BOTANICAL NAME: *Ilex crenata* 'Soft Touch'
CHARACTERISTICS: A dense, mounded, evergreen shrub with soft-textured, glossy green leaves.
SIZE: 24"H x 36"W
SEASONAL INTEREST: Black ornamental berries persist throughout winter.



COMMON NAME: Little Henry Sweetspire

BOTANICAL NAME: *Itea virginica* 'Spirch' USPP 10,988

CHARACTERISTICS: Small native deciduous shrub with a round broad-spreading form.

Size: 24"H x 36"W

SEASONAL INTEREST: Tiny white flowers in cylindrical, drooping racemes in spring. Red, orange, and gold in fall.



COMMON NAME: Crimson Fire Loropetalum

BOTANICAL NAME: *Loropetalum chinense* var. *rubrum* 'PIILC-I' PP 25,534

CHARACTERISTICS: Broad-leaved evergreen shrub with round form.

SIZE: 42"H x 60"W

SEASONAL INTEREST: Pink blooms in spring.



COMMON NAME: Autumn Chiffon Azalea

BOTANICAL NAME: *Rhododendron* 'Robled' PP 15862

CHARACTERISTICS: Compact evergreen shrub with a rounded form.

SIZE: 30"H x 36"W

SEASONAL INTEREST: Double bloom pale pink flowers in spring and fall.

GRASSES + PERENNIALS



COMMON NAME: Lady's Mantle
BOTANICAL NAME: *Alchemilla mollis*
CHARACTERISTICS: Herbaceous clumping perennial.
SIZE: 18"H x 36"W
SEASONAL INTEREST: Tiny, star-shaped, chartreuse flowers in summer.



COMMON NAME: Tickseed
BOTANICAL NAME: *Coreopsis auriculata* 'Nana'
CHARACTERISTICS: Herbaceous perennial growing in a dense, slow-spreading clump.
SIZE: 18"H x 36"W
SEASONAL INTEREST: Yellow daisy-like flowers in late spring to early summer.



COMMON NAME: Purple Cone Flower
BOTANICAL NAME: *Echinacea purpurea*
CHARACTERISTICS: Clump forming native perennial
SIZE: 24"H x 36"W
SEASONAL INTEREST: Showy, daisy-like, purple flowers in summer



COMMON NAME: Blue Flag Iris
BOTANICAL NAME: *Iris versicolor*
CHARACTERISTICS: Clump-forming native perennial, naturalizing.
Size: 24"H x 36"W
SEASONAL INTEREST: Violet blue blooms in late spring and early summer.



COMMON NAME: Pink Muhly Grass
BOTANICAL NAME: Muhlenbergia capillaris
CHARACTERISTICS: Native ornamental grass.
SIZE: 36"H x 24"
SEASONAL INTEREST: Showy pink flower panicles, dark green foliage.



COMMON NAME: Walker's Low Catmint
BOTANICAL NAME: Nepeta x faassenii 'Walker's Low'
CHARACTERISTICS: Clump-forming perennial grown in well-drained soils.
SIZE: 24"H x 30"W
SEASONAL INTEREST: Showy periwinkle blue flower spikes adorn fragrant mounds of gray-green foliage.



COMMON NAME: Cassian Fountain Grass
BOTANICAL NAME: Pennisetum alopecuroides 'Cassian'
CHARACTERISTICS: Warm season graceful flowering ornamental grass.
SIZE: 24"H x 24"W
SEASONAL INTEREST: Showy, silver to pinkish-white, bottle brush shaped flower spikes.



COMMON NAME: May Night Salvia
BOTANICAL NAME: Salvia x sylvestris 'May Night'
CHARACTERISTICS: Upright, clump-forming perennial.
SIZE: 24"H x 36"W
SEASONAL INTEREST: Showy spikes of deep violet-blue flowers in spring and summer.



COMMON NAME: Little Bluestem
BOTANICAL NAME: Schizachyrium scoparium 'Standing Ovation'
CHARACTERISTICS: Native upright, silver-blue grass
Size: 36"H x 24"
SEASONAL INTEREST: Attracts wildlife, bronze-orange fall color.

GROUNDCOVERS



COMMON NAME: Blue Carpet Juniper
BOTANICAL NAME: *Juniperus horizontalis* 'Wiltonii'
CHARACTERISTICS: Low maintenance evergreen groundcover.
SIZE: 6"H
SEASONAL INTEREST: Green to blue-green foliage year-round.



COMMON NAME: Creeping Liriope
BOTANICAL NAME: *Liriope spicata*
CHARACTERISTICS: Herbaceous, grass-like groundcover.
SIZE: 18"H
SEASONAL INTEREST: Lavender flower spikes in late summer to early fall.



COMMON NAME: Asiatic Jasmine
BOTANICAL NAME: *Trachelospermum asiaticum*
CHARACTERISTICS: Low spreading groundcover that grows in average soils.
SIZE: 12" - 18"H
SEASONAL INTEREST: Spreading evergreen forming a dense mat of dark green foliage.

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HARDSCAPE PALETTE

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Image courtesy of Landscape Forms: Concord Collections

Seating

A key component of a successful downtown is ample and consistent seating in the public realm. During the public outreach process community members preferred that seating be a single design, is durable and long-lasting as well as comfortable while not accommodating pedestrians to lie down.

Implementation

Seating can be located in the furniture zone of streetscapes as well as in public spaces like Court Square, green spaces, and greenspaces.

Specifications

Product: Melville Bench, Concord Collection

Manufacturer: Landscape Forms

Color: Stormcloud Powdercoat

Size: 6 feet

Finish: Backed, Aluminum Seat Panel





Image courtesy of Landscape Forms: Concord Collections

Refuse

Ample refuse and recycle receptacles are essential to the cleanliness of downtown. The receptacles must complement the other streetscape elements, be simple from a user standpoint, and be easy for refuse collection workers to empty.

Implementation

Receptacles should be located in the furnishing zone or curb extension of streetscapes as well as in public spaces like Court Square, green spaces, and greenspaces.

Specifications

Product: Poe Litter Receptacle, Concord Collection

Manufacturer: Landscape Forms

Color: Stormcloud Powdercoat

Size: 6 feet

Finish: Slot Opening





Image courtesy of Dero: Hoop Rack Heavy Duty

Bike Rack

Bicycle racks provide secure, dedicated space for bicycle parking. They will encourage active transportation and recreation downtown and keep bicycle parking off street trees and light poles.

Implementation

Bicycle parking can be located in the furnishing zone or in a curb extension. If more than one rack, they should be spaced a minimum of three-foot apart. Racks can either be installed by surface or in-ground mount.

Specifications

Product: Hoop Rack Heavy Duty

Manufacturer: Dero

Color: Iron Grey Powder Coat

Notes: In Ground Mount



Dero: Hoop rack heavy duty surface mount.



Image courtesy of Holophane

Street Lighting

Lighting has the ability to define a street and set the tone for comfort and safety. Pedestrian and street lighting should be consolidated on a single pole. Banners, hanging baskets, and electrical outlets can be incorporated for event needs.

Implementation

Lighting should be broken into two scales, vehicular and pedestrian, and spaced every 60 to 75 feet apart from one another based on desired affect. The first light should be set back 15 feet from the intersection and the remaining spaced as equidistant as possible.

Specifications

Product: GlasWerks LED Prismatic Glass, Prague

Manufacturer: Holophane

Color: Stormcloud Powdercoat

Notes: Teardrop class, Arm Mount





Catenary lighting on main street in Sulphur Spring, Texas

Catenary Lighting

Catenary lighting, or more commonly known as string lighting, provides wonderful ambiance to a street, increased safety, and encourages nightlife. The lighting helps further define the street as a part of the public realm.

Implementation

Catenary lighting should be installed on both sides of Elm Street. Lighting must not cross Main Street as it is not approved by the North Carolina Department of Transportation. Stainless steel support wire should be spanned from the top of the street light to top of street light both directly across from each other and diagonally.

Specifications

Product: Cafe Lights

Manufacturer: Sterling Lighting

Notes: 300 foot length



Sterling Lighting: Commercial String Lighting

**Clay Brick Paver****Concrete****Asphalt****Granite Cobble****Concrete Paver****Core-Ten Steel**

Hardscape Materials

Currently, Graham's hardscape palette in the public realm downtown consists of concrete, asphalt, and small areas of clay brick. While these materials are traditional streetscape materials, in their current configuration, they do not contribute to the place that is downtown. Character must be brought into the public realm hardscape through natural materials, pattern, and orientation.

The material palette moving forward will complement what is existing while adding natural stone, weathered steel, and clay brick pavers. Concrete sidewalks will be maintained with a 6'x6' diamond pattern scored in the sidewalk frontage and clear zone. The furnishing zone will become a three-piece concrete paver. The roadway in most places will stay asphalt; however, on Elm Street and within Court Square it will become clay brick paver. Accents at crosswalks, bulb-outs, and curb extensions will be coreten steel and natural granite. This new palette will compliment Graham's historic buildings and bring life into downtown.



Standard Crosswalk



Enhanced Crosswalk

Crosswalks

Crosswalks are an important element to establish a safe pedestrian environment within a streetscape. They help inform pedestrians where to cross as well as inform drivers with a visual cue that pedestrians may be present at this location. Crosswalks must be present at all intersections within downtown and may be necessary at midblock crossings with heavy pedestrian movements. Pedestrian safety in Graham will benefit from a more consistent approach to crosswalk treatments. The following types of crosswalks should be used throughout downtown.

Standard Crosswalk

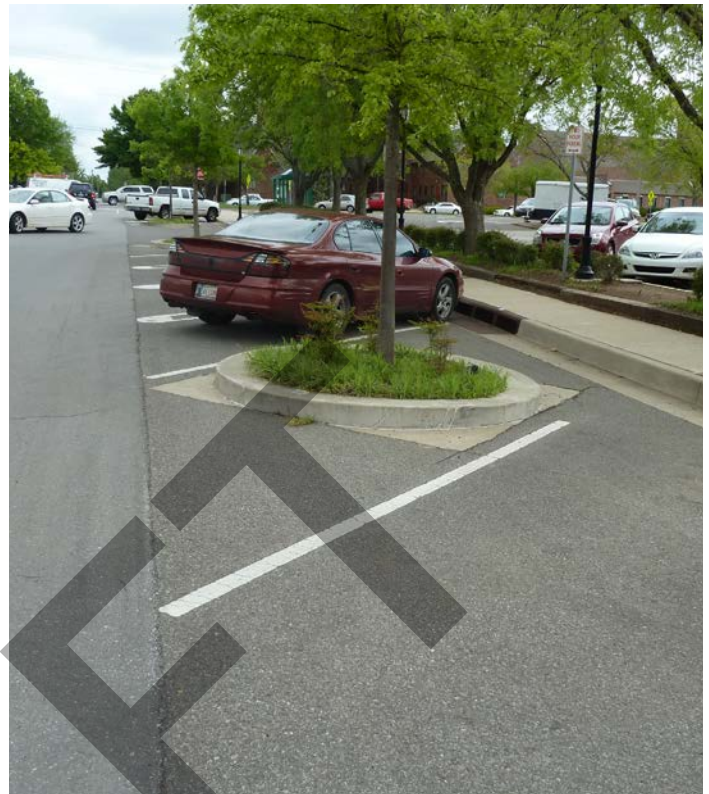
The standard crosswalk treatment should be a continental or high visibility crosswalk. The high visibility crosswalk typically consists of 24" wide white bars spaced 4' on center perpendicular to the path of travel. The crosswalk should be at least 8' wide or the width of the approaching sidewalk if it is greater. In areas of heavy pedestrian volumes, crosswalk can be up to 16' wide.

Enhanced Crosswalk

The enhanced crosswalk treatment should be a brick crosswalk with highly contrasting colors from the roadway material. The enhanced crosswalk should consist of two 12" decorative concrete bands with brick placed between them. The crosswalk should be at least 10' wide or the width of the approaching sidewalk if it is greater. In areas of heavy pedestrian volumes, crosswalk can be up to 18' wide.



Parallel Parking



Angled Parking

On-Street Parking

Parking is essential in creating a vibrant and accessible downtown. On-street parking helps provide easy access to storefronts, street side activates, green spaces, and reduces travel speeds. The parking has an additional benefit of providing a greater buffer and physical barrier between the roadway and sidewalk. On-street parking should not encroach the crosswalk or impede visibility at intersections. For this reason, all parking must maintain a minimum of a 20 foot buffer from crosswalks and intersections.

Standards

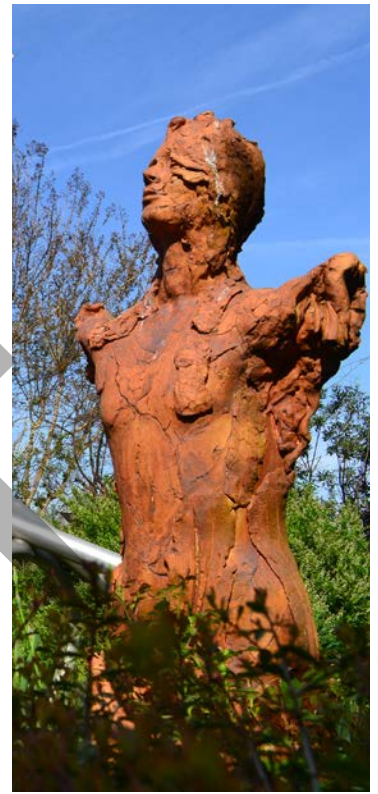
Standard parallel on-street parking stalls should be 8 feet in width, including the gutter pan and to the centerline of striping, and 20 feet in length. This will accommodate most standard-size vehicles and trucks. Special attention should be given to people with disabilities. The same dimensions should be used for the physical parking space with the addition of a 5 feet buffer added to the front or rear of the space for additional room to maneuver.

Standard angled parking stalls should be 9 feet in width and 18 feet in length, from the center of the valley gutter to the face of curb. This will accommodate most standard-size vehicles and trucks. Consult the American’s with Disabilities Act for additional parking requirements.

Head-Out Angled Parking

Some citizens have expressed interest in head-out or reverse angle parking. Benefits to consider include:

- Improved driver visibility when exiting the stall
- Car loading is performed from the sidewalk
- Car doors funnel children to the sidewalk
- The first step to parallel parking
- May aid in traffic calming



Existing art in Downtown Graham

Public Art

Downtown Graham is already the scene of a great public art program. Alamance Arts has played a key role encouraging art to be weaved into the fabric of Graham. Public art has been effective in the economic revitalization of downtown's as well as the creation of a sense of place. Known benefits include education, job creation, increased real estate value and increased tourism.

Place for Public Art

In the public realm, public art can be located on streets, street corners and bulbouts near intersections, in the center of roundabouts, and in parks and plazas. Business owners can also allow for the installation or display of various types of art both indoors and outdoors.

Types of Public Art

Public art has endless opportunities for innovation and the medium in which they are made. The following would be ideas that could be easily incorporated into the public realm of Downtown Graham:

- Decorating utilities boxes
- Sculptures
- Planters, green walls, and hanging baskets
- Restoration of original business murals
- Temporary installations
- Interactive art
- Memorials

**Modern Bench and Refuse****Modern Bench Pods****Artistic Bench****Big Belly Refuse****Artistic Bike Rack****Architectural Bench**

Alternative Furnishings

The City of Graham currently lacks a standard for furnishings within downtown. A standard helps establish a sense of place and ownership of maintenance. While the preceding pages suggested a standard that is contemporary in nature, alternative furnishings that are more artistic and modern may be considered.

The above alternative furnishings are just a taste of what is currently available to municipalities. Custom artistic bike racks currently exist in downtown at multiple locations (pictured above). Big Belly refuse containers have the ability to dawn custom artwork and have the ability to use solar power for trash compaction. Alternative furnishings should be evaluated on a case by case basis and fit the aesthetic context of Graham.

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APPENDIX B

COST ESTIMATES

Court Square Pilot Restripe
Elm Street 100 Block
Downtown Park Design Concept



COST ESTIMATES

The following pages of appendices provide opinions of probable cost for phase one projects and public realm elements within Downtown Graham, North Carolina. The opinion of probable costs are order-of-magnitude estimates made for budget purposes only. Estimates shall be reviewed, revised, and adjusted accordingly at program verification/schematic design phases. The estimates assume a competitive bid and are opinions of probable construction cost based on fair market value, historical NCDOT bid tabulations, and estimator's judgement. The estimates do not include right-of-way acquisition, utility relocation, or design fee. This is not a prediction of the anticipated low bid and should be used for planning purposes only. For each fiscal year after the publication of this document, 2019, estimator should add 5% to the unit cost of each item.

Opinion of probable costs included:

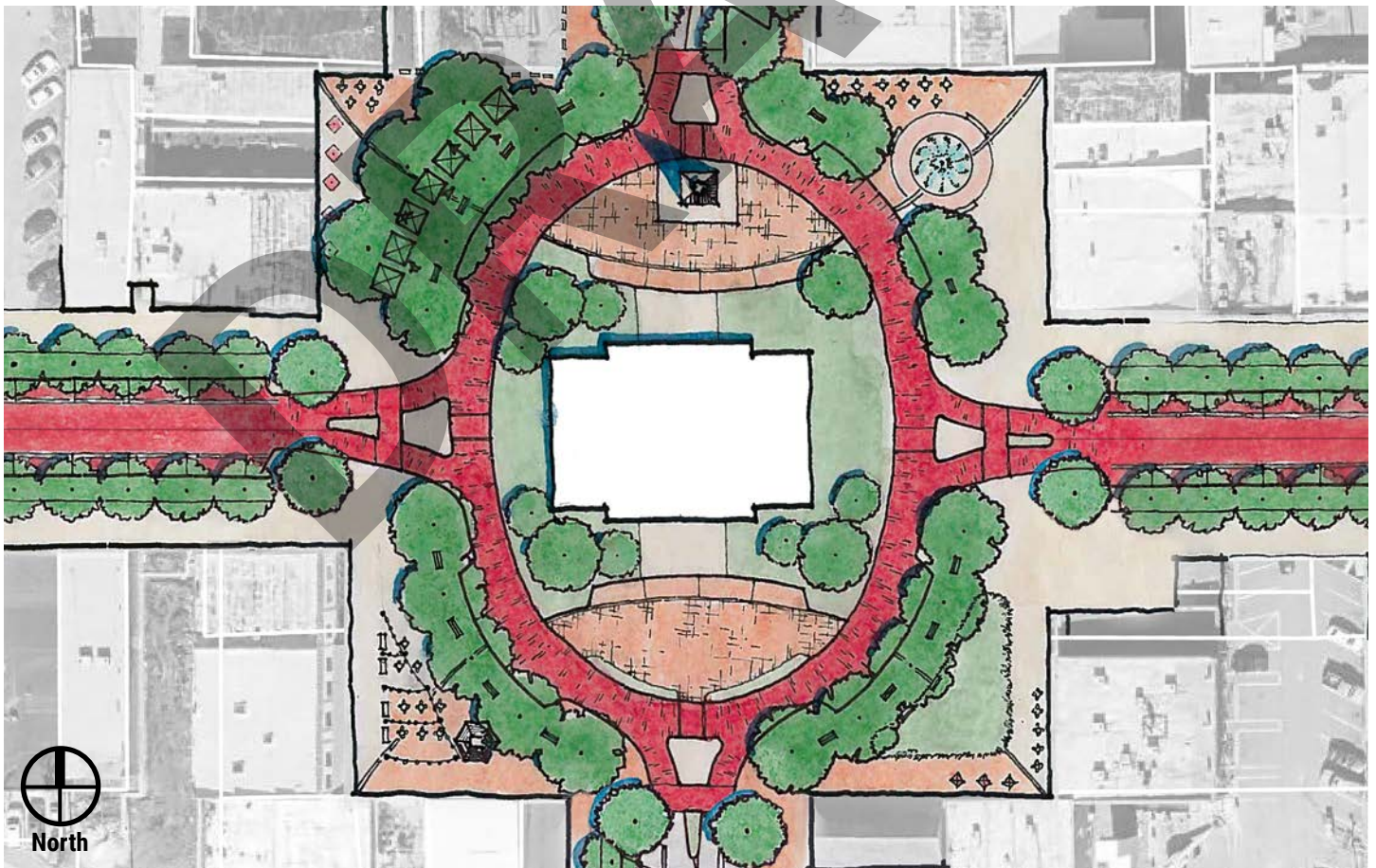
- Phase One: Court Square Pilot Restripe
- Phase One: Elm Street Reconstruction
- Phase One: Downtown Park Design Concept

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Phase One: Court Square Pilot Restripe

Planning Level Opinion of Probable Construction Cost

Through the implementation of a pilot restripe project residents and visitors alike can experience the vision for Court Square first hand. Street space will be reallocated based on the concept plan through temporary paint, planters, and barriers. The opinion of probable costs on the preceding page are order-of-magnitude estimates made for budget purposes only. Estimates shall be reviewed, revised, and adjusted accordingly at program verification/schematic design phases. For each fiscal year after the publication of this document, 2019, estimator should add 5% to the unit cost of each item.



Proposed concept design for Court Square

Phase One: Court Square Pilot Restripe

| DESCRIPTION | QUANTITY | UNIT | UNIT COST | ITEM COST |
|---|----------|------|-----------|--------------------|
| PAINT PAVEMENT MARKINGS (4" WHITE) | LF | 4712 | \$0.25 | \$1,178.00 |
| PAINT PAVEMENT MARKINGS (8" WHITE) | LF | 1152 | \$0.50 | \$576.00 |
| PAINT PAVEMENT MARKINGS (24" WHITE) | LF | 360 | \$2.10 | \$756.00 |
| PAINT PAVEMENT MARKINGS (SYMBOL) | EA | 16 | \$40.00 | \$640.00 |
| CONCRETE PLANTERS | EA | 20 | \$250.00 | \$5,000.00 |
| PORTABLE CONCRETE BARRIER | LF | 750 | \$30.00 | \$22,500.00 |
| SUBTOTAL | | | | \$30,650.00 |
| CONTINGENCY (30%) | | | | \$9,195.00 |
| MOBILIZATION (5%) | | | | \$1,533.00 |
| TEMPORARY TRAFFIC CONTROL (5%) | | | | \$4,598.00 |
| SIGNING (2%) | | | | \$613.00 |
| CONCEPT LEVEL OPINION OF PROBABLE COST TOTAL | | | | \$46,600.00 |
| PRECONSTRUCTION ENGINEERING COST | | | | \$8,000.00 |

■ Inflation of 5% per year ■

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Phase One: Elm Street Reconstruction

Planning Level Opinion of Probable Construction Cost

Elm Street will serve as the City's primary festival street. Phase One encompasses both East and West Elm Street within the 100 block of downtown Graham. The project will focus on public realm improvements along the roadway and at both Maple Street and Marshal Street intersections (see Chapter 3 Streetscape + Public Space: Elm Street for additional details). The opinion of probable costs on the proceeding page are order-of-magnitude estimates made for budget purposes only. Estimates shall be reviewed, revised, and adjusted accordingly at program verification/schematic design phases. For each fiscal year after the publication of this document, 2019, estimator should add 5% to the unit cost of each item.

■ Inflation of 5% per year ■



Photo simulation of Elm Street looking east toward Court Square

Phase One: Elm Street Reconstruction

| DESCRIPTION | QUANTITY | UNIT | UNIT COST | ITEM COST |
|---|----------|-------|-------------|-----------------------|
| PREPARING ROW | STA | 7 | \$2,500.00 | \$17,500.00 |
| CONSTRUCTION STAKING, LINES, AND GRADES | LS | 1 | \$15,000.00 | \$15,000.00 |
| CLEARING & GRUBBING | AC | 1.5 | \$25,000.00 | \$37,500.00 |
| TREE REMOVAL | EA | 12 | \$500.00 | \$6,000.00 |
| UNCLASSIFIED EXCAVATION | CY | 1150 | \$50.00 | \$57,500.00 |
| BORROW EXCAVATION | CY | 200 | \$35.00 | \$7,000.00 |
| REMOVAL OF EXT ASPHALT PAVEMENT | SY | 4607 | \$9.00 | \$41,465.00 |
| REMOVAL OF EXT CONCRETE PAVEMENT | SY | 1600 | \$9.00 | \$14,400.00 |
| CONCRETE DRIVEWAY REMOVAL | EA | 6 | \$1,000.00 | \$6,000.00 |
| AGGREGATE BASE COURSE | TON | 1378 | \$52.00 | \$71,679.23 |
| CONCRETE CURB & GUTTER (2'-0) | LF | 794 | \$52.00 | \$41,288.00 |
| CONCRETE CURB - VALLEY GUTTER | LF | 672 | \$35.00 | \$23,520.00 |
| 6" CONCRETE SIDEWALK | SY | 2511 | \$56.00 | \$140,597.33 |
| 6" CONCRETE DRIVEWAY | SY | 98 | \$120.00 | \$11,733.33 |
| CONCRETE CURB RAMPS | EA | 16 | \$2,225.00 | \$35,600.00 |
| FURNISHING ZONE PAVERS | SF | 14894 | \$18.00 | \$268,092.00 |
| FURNISH AND INSTALL - BRICK ROADWAY | SY | 2522 | \$200.00 | \$504,444.44 |
| 24" RCP CULV CLASS IV | LF | 200 | \$105.00 | \$21,000.00 |
| 36" RCP CULV CLASS IV | LF | 725 | \$175.00 | \$126,875.00 |
| CURB INLET | EA | 8 | \$2,500.00 | \$20,000.00 |
| INSTALL TOPSOIL | CY | 100 | \$90.00 | \$9,000.00 |
| LANDSCAPING | LS | 1 | \$20,000.00 | \$20,000.00 |
| IRRIGATION | LS | 1 | \$15,000.00 | \$15,000.00 |
| PEDESTRIAN LIGHTING | EA | 20 | \$2,500.00 | \$50,000.00 |
| LANDSCAPE LIGHTING | EA | 96 | \$250.00 | \$24,000.00 |
| LANDSCAPE TRANSFORMER | EA | 1 | \$650.00 | \$650.00 |
| LANDSCAPE AMENITY (BENCH) | EA | 16 | \$2,500.00 | \$40,000.00 |
| LANDSCAPE AMENITY (REFUSE) | EA | 8 | \$1,500.00 | \$12,000.00 |
| SUBTOTAL | | | | \$1,620,344.34 |
| CONTINGENCY (30%) | | | | \$486,104.00 |
| MOBILIZATION (5%) | | | | \$81,018.00 |
| TEMPORARY TRAFFIC CONTROL (5%) | | | | \$81,018.00 |
| MISCELLANEOUS SANI SEVER/WATER UTILITIES (25%) | | | | \$405,087.00 |
| ELECTRICAL SERVICES (5%) | | | | \$81,018.00 |
| SIGNING (1.0%) | | | | \$16,204.00 |
| PAVEMENT MARKINGS (1.0%) | | | | \$16,204.00 |
| CONCEPT LEVEL OPINION OF PROBABLE COST TOTAL | | | | \$2,787,000.00 |
| PRECONSTRUCTION ENGINEERING (12%) | | | | \$334,440.00 |
| CONSTRUCTION ENGINEERING AND INSPECTION (10%) | | | | \$278,700.00 |

Phase One: Downtown Park Design Concept: Sesquicentennial Park

Planning Level Opinion of Probable Construction Cost

A redesign of the Sesquicentennial Park could provide an additional amenity to the Court Square. The opinion of probable costs on the proceeding page are order-of-magnitude estimates made for budget purposes only. Estimates shall be reviewed, revised, and adjusted accordingly at program verification/schematic design phases. For each fiscal year after the publication of this document, 2019, estimator should add 5% to the unit cost of each item.

If a different location is desired, the opinion of probable construction cost can still be used with some alterations needed based on a sites comparable size and scale.



Conceptual rendering of Court Square Park

Phase One: Sesquicentennial Park Redesign

| DESCRIPTION | QUANTITY | UNIT | UNIT COST | ITEM COST |
|---|----------|------|-------------|---------------------|
| MOBILIZATION | LS | 1 | \$10,000.00 | \$10,000.00 |
| CONST. STAKES, LINES, & GRADES | EA | 1 | \$5,000.00 | \$5,000.00 |
| TRAFFIC CONTROL | LS | 1 | \$2,500.00 | \$2,500.00 |
| CLEARING AND GRUBBING | LS | 1 | \$2,500.00 | \$2,500.00 |
| SILT FENCE | LF | 240 | \$5.00 | \$1,200.00 |
| ASPHALT REMOVAL | SY | 605 | \$10.00 | \$6,050.00 |
| UNSUITABLE SOIL EXCAVATION | CY | 100 | \$30.00 | \$3,000.00 |
| GENERAL GRADING AND GROUND PREPARATION | LS | 1 | \$5,000.00 | \$5,000.00 |
| GRADED AGGREGATE BASE COURSE | TON | 102 | \$26.00 | \$2,652.00 |
| DRAINAGE | LS | 1 | \$16,000.00 | \$16,000.00 |
| WOVEN GEOTEXTILE FABRIC | SF | 2590 | \$10.00 | \$25,900.00 |
| SLATE CHIP PATIO | TON | 45 | \$75.00 | \$3,375.00 |
| CONCRETE UNIT PAVER | SF | 1328 | \$18.00 | \$23,904.00 |
| WATER FEATURE - CENTER | LS | 1 | \$78,000.00 | \$78,000.00 |
| WATER FEATURE - SEAT WALL | LS | 2 | \$45,000.00 | \$90,000.00 |
| PLANTERS/SEATWALL | LF | 124 | \$250.00 | \$31,000.00 |
| TOPSOIL | CY | 40 | \$50.00 | \$2,000.00 |
| LANDSCAPE GRADING AND PREPARATION | LS | 1 | \$1,500.00 | \$1,500.00 |
| LANDSCAPE PLANTING | LS | 1 | \$3,280.00 | \$3,280.00 |
| IRRIGATION INSTALLATION | LS | 1 | \$3,000.00 | \$3,000.00 |
| PEDESTRIAN LIGHTING | EA | 2 | \$1,800.00 | \$3,600.00 |
| LANDSCAPE LIGHTING | LS | 1 | \$9,175.00 | \$9,175.00 |
| PERGOLA | LS | 1 | \$45,000.00 | \$45,000.00 |
| CAFÉ TABLE - 4 TOP | EA | 8 | \$3,000.00 | \$24,000.00 |
| TRASH RECEPTACLE | EA | 2 | \$1,200.00 | \$2,400.00 |
| SWING BENCH | EA | 4 | \$2,500.00 | \$10,000.00 |
| SUBTOTAL | | | | \$400,036.00 |
| CONTINGENCY (30%) | | | | \$120,010.80 |
| UTILITY RELOCATION(5%) | | | | \$20,001.80 |
| CONCEPT LEVEL OPINION OF PROBABLE COST TOTAL | | | | \$520,100.00 |
| PRECONSTRUCTION ENGINEERING (12%) | | | | \$62,412.00 |
| CONSTRUCTION ENGINEERING AND INSPECTION (10%) | | | | \$52,010.00 |

■ Inflation of 5% per year ■

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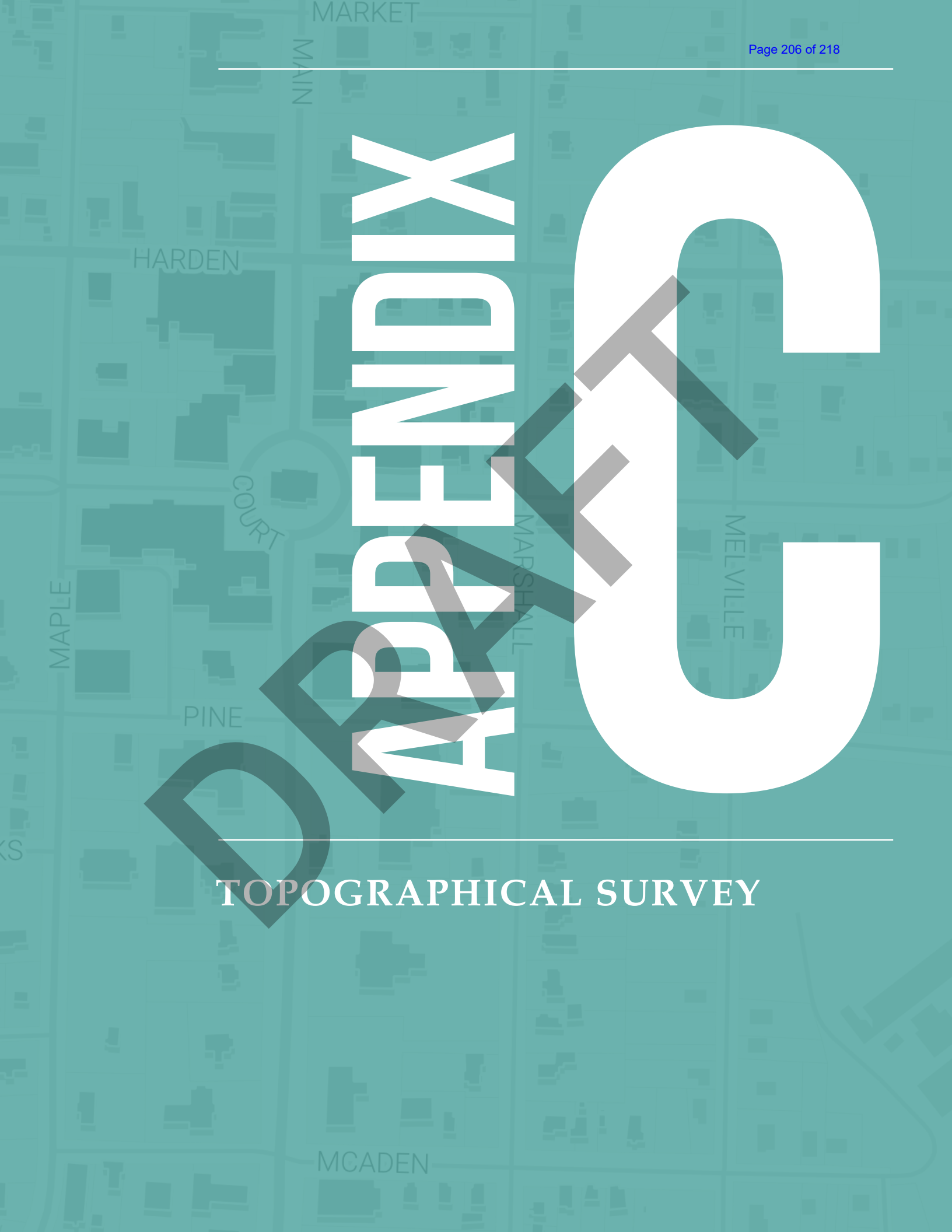
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APPENDIX C

TOPOGRAPHICAL SURVEY

Topographical Survey

In an effort to help the City of Graham move planning projects forward to design, Toole Design hired a surveyor to develop a topographical survey of the study area. A topographical survey gathers data about the natural and man-made features of a particular site (e.g. building footprints, utility locations, edge of pavement, elevations, sidewalks, vegetation, property ownership, etc.). All design projects start with a survey and Graham now has the ability to move forward on projects within the study area. The survey should be updated periodically as Graham begins implementing recommendations downtown. Since April of 2019, the survey has already assisted two downtown business owners/property owners with utility and property boundary inquiries. An excerpt of the survey can be found on the adjacent page.

The survey excerpt is for reference/information purposes only. Residents are encouraged to contact the City of Graham for specific questions regarding the survey.

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STAFF REPORT

| | |
|--------------|---------------------------------------|
| SUBJECT: | ORDINANCE TO AMEND EXCAVATION PERMIT |
| PREPARED BY: | AARON HOLLAND, ASSISTANT CITY MANAGER |

REQUESTED ACTION:

Amend Code of Ordinances to amend requirements and permit for excavation in the City Right-of-Way.

BACKGROUND/SUMMARY:

The City of Graham currently has a process described in the Code of Ordinances that requires a permit for anyone wanting to make any opening in any street or sidewalk. This has proven to be insufficient due to utility work being performed not necessarily within the street or sidewalk, but along these areas within the right-of-way. This has created issues with our Public Works and Utilities departments by not being properly informed of work being done and ultimately causing conflicts in the field delaying projects or damaging City infrastructure.



Staff has researched communities to evaluate language that would require a permit for any work within the City right-of-way as well as create an application that would be sufficient for the work performed.

FISCAL IMPACT:

Nominal. Staff time will be required to review and process application requests.

STAFF RECOMMENDATION:

Approval. City staff recognizes the importance of eliminating underground conflicts for the safety of the citizens and development community.

SUGGESTED MOTION(S):

I move to approve the Ordinance amendment to CHAPTER 18- STEETS, SIDEWALKS AND OTHER PUBLIC PLACES, ARTICLE IV- EXCAVATION PERMIT of the Code of Ordinances to amend the requirements and permitting process for excavation in the City Right-of-Way.

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF GRAHAM, AMENDING CHAPTER 18, ARTICLE IV, SEC. 18-106 TO 18-107 TO THE CODE OF ORDINANCES OF THE CITY OF GRAHAM, NORTH CAROLINA

The City Council of the City of Graham, North Carolina, does ORDAIN:

Sec. 1. That the Code of Ordinances, City of Graham, North Carolina, is hereby amended by amending sections 18-106 to 18-107, which said section read as follows:

Chapter 18 – STREETS, SIDEWALKS AND OTHER PUBLIC PLACES

ARTICLE IV. – EXCAVATION PERMIT

EXISTING:

Sec. 18-106. - Required.

No person shall make any opening in any street or sidewalk of the city without a permit therefor issued by the chief code enforcement officer by authority of the city manager, provided that a permit shall not be required where such work is performed under a contract with the city, but in the event that it becomes necessary to such work that a sidewalk or street must be wholly or partially obstructed, the person shall notify the fire department before obstructing the sidewalk or street, unless prevented by a sudden emergency.

(Code 1978, ch. F, art. IV, § 2; Code 1983, § 12-87)

Sec. 18-107. - Application.

Any person desiring to make any opening in any street or sidewalk shall make written application therefor, which application shall show the location of the proposed opening, the purpose thereof and the approximate number of square yards of surface to be cut. Such application shall be accompanied by the fee required by the following section; provided the city manager may enter into an agreement with any utility company to be invoiced on a monthly basis.

(Code 1978, ch. F, art. IV, § 3; Code 1983, § 12-88)

PROPOSED:

Sec. 18-106. - Required.

No person shall make any **excavation or opening or dig any ditch, trench, tunnel or hole in, along, across or under in any** street, sidewalk **or other public place for the purpose of laying or placing therein any pipe, wire or poles for any other purposes, unless a written permission therefor has been issued by the City by authority of the city manager,** provided that a permit shall not be required where such work is performed under a contract with the city, but in the event that it becomes necessary to such work that a sidewalk or street must be wholly or partially obstructed, the person shall notify the **Inspections, Public Works, Police** and Fire departments **at least two hours** before obstructing the sidewalk or street, unless prevented by a sudden emergency.

(Code 1978, ch. F, art. IV, § 2; Code 1983, § 12-87)

Sec. 18-107. - Application.

Any person desiring to make any opening in any street or sidewalk shall make written application therefor, which application shall show the location of the proposed opening, the purpose thereof and the approximate number of square yards of surface to be cut. **Any new installation shall include drawings illustrating proposed location and depth.** Such application shall be accompanied by the fee required by the following section; provided the city manager may enter into an agreement with any utility company to be invoiced on a monthly basis.

(Code 1978, ch. F, art. IV, § 3; Code 1983, § 12-88)

Sec. 2. That this Ordinance shall be in full force and effect from and after its passage, approval and publication, as provided by law

This the _____ day of _____, 2019.

Mayor

ATTEST:

City Clerk



Permit Application for Work on City Right-Of-Way

Issue Date: _____

Expiration Date: _____

Contractor: _____

Work Location: _____

Proposed Start Date: _____

Proposed Work Duration: _____

(Street Address)

Certificate #: _____

Between: _____

Contact Name: _____

And: _____

Work Phone: _____

Street Closure Required? Yes No

Mobile Phone: _____

(If yes, attach proposed detour plan, including detour routes and detour signage.)

Fax Number: _____

Work Type: _____

Work Description:

**Please submit drawings illustrating proposed location and depth for any new installations.*

Unless special notice has been given at issuance of the permit, the permittee certifies that there will be no danger of collapse, explosion, or underground damage in the course of the project.

Permittee to the fullest extent permitted by law shall indemnify, defend, and hold the City harmless from and against any and all claims, liabilities, damages, injuries, losses, costs, and expenses of any kind (including attorneys' fees and court costs) incurred by the City arising from suits, claims, and/or causes of action arising from or growing out of actions or inactions on the part of permittee.

The Public Works Department shall be contacted at 336-570-6709 between 7:00am - 4:00pm Monday through Friday prior to and at the conclusion of any construction.

In emergency situations the permittee shall immediately notify the City Manager and the Fire or Police Chief of the emergency event. The permittee may proceed to take whatever actions are necessary to immediately respond to the emergency. Within two (2) business days after the occurrence of the emergency the permittee shall apply for the Permit to Work on City Right-Of-Way.

If encroachment onto private property is necessary for construction, staging, vehicle/equipment storage, etc. activities then the permittee is directed to contact and obtain permission from said property owners for such encroachment.

A COPY OF AN UP TO DATE PERMIT MUST BE ON THE JOB SITE AT ALL TIMES.

APPROVED (BY THE CITY OF GRAHAM)

I HAVE READ AND UNDERSTAND THE ABOVE

Signature

Date

Applicants Signature

Date

STAFF REPORT

| | |
|--------------|---|
| SUBJECT: | ORDINANCE TO REMOVE STORAGE REQUIREMENT FOR JUNK DEBRIS |
| PREPARED BY: | AARON HOLLAND, ASSISTANT CITY MANAGER |

REQUESTED ACTION:

Amend Code of Ordinances to delete requirement of the City to store removed junk debris for seven days.

BACKGROUND/SUMMARY:

Code Enforcement plays an important role in preserving the attractive appearance of neighborhoods in the City of Graham. The mission of Code Enforcement is to provide fair and comprehensive enforcement of the City's Code of Ordinances, which include junk debris, trash, weeds, and minimum housing. Although the Code of Ordinances provides the process for enforcement of violations, there comes a time when the Code needs amending to afford more effective compliance.

After evaluating the current language, City staff is recommending amending Sec. 12-71 (1) which references the abatement of nuisances by removal of junk debris. The requested amendment is to delete an antiquated requirement of the City to store removed items for seven days. This will allow for a more effective abatement process of junk debris removal from properties refusing to comply by allowing the option for the City to dispose of junk nuisances more expeditiously.

FISCAL IMPACT:

TBD. Abatement costs incurred by the City are typically assessed against the property owner.

STAFF RECOMMENDATION:

Approval. City staff recognizes the importance of effective language within the Code of Ordinances as a mechanism to fulfill expectations placed on staff by the citizens. Removing this requirement expands the flexibility for staff to work more efficiently.

SUGGESTED MOTION(S):

I move to approve the Ordinance amendment to CHAPTER 12- OFFENSES AND MISCELLANEOUS PROVISIONS, ARTICLE III- NUISANCES of the Code of Ordinances to delete requirement of the City to store removed junk debris for seven days.

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF GRAHAM, AMENDING CHAPTER 12, ARTICLE III, SEC. 12-71 TO THE CODE OF ORDINANCES OF THE CITY OF GRAHAM, NORTH CAROLINA

The City Council of the City of Graham, North Carolina, does ORDAIN:

Sec. 1. That the Code of Ordinances, City of Graham, North Carolina, is hereby amended by amending section 12-71 (1), which said section read as follows:

Chapter 12 – OFFENSES AND MISCELLANEOUS PROVISIONS

ARTICLE III. – NUISANCES

Sec. 12-71. - Administration.

The inspector or other designee of the city manager shall be responsible for the administration and enforcement of this article. Nothing within this article shall be construed to limit the legal authority of the city police department and fire department in enforcing other laws or in otherwise carrying out their duties.

- (1) *Enforcement.* The enforcement officer of this article shall have the authority to enforce the terms of these provisions by issuance of a citation, by ordering the property owner to abate the nuisance, by taking action to abate the nuisance or by filing an action in court to abate such nuisance, or any combination of these methods. If the nuisance is abated by the city, the costs of such abatement shall be assessed against the property owner and be collected the same as delinquent taxes. ~~The owner of any property removed from the premises shall have seven days from removal to claim such property.~~
- (2) *Contents of notice.* The notice to abate the nuisance shall contain:
 - a. The description of the nuisance.
 - b. Statement of acts necessary to abate the nuisance.
 - c. An order to abate the nuisance within a stated time, not to exceed 30 days.
 - d. Penalty for violation of the nuisance.
 - e. Right of appeal of the determination.
- (3) *Service of notice.*
 - a. Upon determination that a violation of a provision of this article has occurred, the enforcement officer shall cause a written notice to persons known to the city to be in control of the premises and the owner of the property as shown by the ad valorem tax records of the city by personal service or by registered or certified mail, return receipt requested. If such notice is refused or is returned unclaimed, then said property shall be posted with notice. If the name of the owner of the property cannot be ascertained, then the notice shall be served on any person in possession of the subject property or, if there is no person in possession of it, by posting the notice on the subject property.
 - b. Any such notice may be served by any authorized representative of the city or by any police officer of the city.
- (4) *Penalties.*

- a. Violation of any provision of this article shall subject the offender to a civil penalty in the amount of \$50.00 to be recovered by the city in a civil action in the nature of a debt if the offender does not pay the penalty within 30 days of being cited.
 - b. Each day that a violation continues to exist 30 days after issuance of the original citation shall constitute a separate and distinct offense without multiple citations being issued and shall be subject to additional civil penalties of \$50.00 per day.
 - c. The amount of the penalty, if not paid within 30 days of issuing of notice, may be collected in the same manner as provided for the collection of delinquent taxes.
 - d. In addition to or in lieu of the collection of the penalty, the administrator may seek redress through the courts to abate such nuisance by injunctive relief.
 - e. Any costs incurred by the city on enforcing this article by abatement of the nuisance shall be assessed against the property owner and may be collected in the same manner provided for the collection of delinquent taxes.
-

Sec. 2. That this Ordinance shall be in full force and effect from and after its passage, approval and publication, as provided by law

This the _____ day of _____, 2019.

Mayor

ATTEST:

City Clerk





I make a motion to go into Closed Session Pursuant to the Terms of N.C.G.S. §. 143-318-11 (a) (5): to establish, or to instruct the public body's staff or negotiating agents concerning the position to be taken by or on behalf of the City Council in negotiating (i) the price and other material terms of a contract or proposed contract for the acquisition of real property by purchase, option, exchange, or lease.